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**PLANNING COMMISSION
MEETING AGENDA
REGULAR MEETING**

David Lambert, Chairman, Marianna Perakis, Vice Chairman
Toby Buechner, Carlton Faison, Tyler Fox, Michael W. Hutson, Tom Krent,
Lakshmi Malalahalli and John J. Tagle

November 14, 2023

7:00 P.M.

Council Chambers

1. ROLL CALL
2. APPROVAL OF AGENDA
3. APPROVAL OF MINUTES – October 24, 2023
4. PUBLIC COMMENT – For Items Not on the Agenda

SPECIAL USE APPROVAL

5. SPECIAL USE APPROVAL AND PRELIMINARY SITE PLAN REVIEW (SU JPLN2023-0019) - Proposed Clean Express Car Wash, West side of John R, North of Fourteen Mile (PIN 88-20-35-401-001), Section 35, Zoned GB (General Business) District

PRELIMINARY SITE PLAN REVIEW

6. PRELIMINARY SITE PLAN REVIEW (File Number SP JPLN2023-0026) – Proposed Controlled Exit onto Colebrook from Parking Lot, Northwest corner of Rochester and Colebrook (3635 Rochester; PIN 88-20-22-276-051, -052 & -053), Section 22, Currently Zoned GB (General Business) and R-1C (One Family Residential) District

OTHER ITEMS

7. DRAFT MASTER PLAN
8. PUBLIC COMMENT- For Items on the Agenda
9. PLANNING COMMISSION COMMENT
10. ADJOURN

NOTICE: People with disabilities needing accommodations for effective participation in this meeting should contact the City Clerk by e-mail at clerk@troymi.gov or by calling (248) 524-3317 at least two working days in advance of the meeting. An attempt will be made to make reasonable accommodations

Televised Live, Government Channel WTRY (10 WideOpenWest and 17 Comcast) Replayed Wednesdays 3:00 pm, 6:00 pm and 11:00 pm

Chair Lambert called the Regular meeting of the Troy City Planning Commission to order at 7:00 p.m. on October 24, 2023, in the Council Chamber of the Troy City Hall. Chair Lambert and Vice Chair Perakis presented opening remarks relative to the role of the Planning Commission and procedure of tonight’s meeting.

1. ROLL CALL

Present:

- Toby Buechner
- Tyler Fox
- Michael W. Hutson
- David Lambert
- Lakshmi Malalahalli
- Marianna Perakis
- John J. Tagle

Absent:

- Carlton M. Faison
- Tom Krent

Also Present:

- Ben Carlisle, Carlisle Wortman & Associates
- R. Brent Savidant, Community Development Director
- Julie Quinlan Dufrane, Assistant City Attorney
- Kathy L. Czarnecki, Recording Secretary

2. APPROVAL OF AGENDA

Resolution # PC-2023-10-059

- Moved by: Fox
- Support by: Perakis

RESOLVED, To approve the Agenda as prepared.

- Yes: All present (7)
- Absent: Faison, Krent

MOTION CARRIED

3. APPROVAL OF MINUTES – September 26, 2023

Resolution # PC-2023-10-060

- Moved by: Malalahalli
- Support by: Buechner

RESOLVED, To approve the minutes of the September 26, 2023 Regular meeting as submitted.

Yes: All present (7)
 Absent: Faison, Krent

MOTION CARRIED

4. PUBLIC COMMENT – For Items Not on the Agenda

Daniel Bousho, 94 Hickory; addressed residential development as relates to building design and options to rent or purchase, encouraged mortgaged-based communities.

Jeff Williams, 159 Telford; addressed proposed Planned Unit Development (PUD) application located east of Livernois and north of Square Lake. He encouraged members of the Planning Commission to personally reach out to him to visit Telford Ridge subdivision and view the proposed development from the perspective of the subdivision.

Mr. Savidant addressed public notification of the above-referenced proposed PUD application. He clarified review of the application and Public Hearing is scheduled on the November 28, 2023 Planning Commission Regular meeting agenda.

CONDITIONAL REZONING AND SPECIAL USE APPROVAL

5. PUBLIC HEARING – CONDITIONAL REZONING AND SPECIAL USE APPROVAL - (CR JPLN2023-001) – Proposed Jax Kar Wash, North side of Big Beaver, East of Rochester (PIN 88-20-23-401-036), Section 23, From O (Office) to IB (Integrated Industrial and Business) District

Mr. Savidant said public notification on the proposed development was not clear that the intent of the applicant is to rezone the entire parcel to IB (Integrated Industrial & Business), including the portion zoned P (Vehicular Parking). Mr. Savidant said after discussion with the applicant, his legal representative, and the City Attorney, it is determined to postpone the item to the December 12, 2023 Regular meeting to allow proper notification. Mr. Savidant said the City Planning Consultant and the applicant would make their presentations, the Public Hearing would be opened, but there would be no deliberation on the application this evening.

Ms. Dufrane announced new public notices with the correct zoning designations and properties proposed to be rezoned would be mailed to the appropriate property owners. She noted the applicant intends not to develop on the property zoned P (Vehicular Parking) and is voluntarily offering conditions related to the property. Ms. Dufrane said public comments made this evening would be on the record and in the meeting minutes.

Mr. Carlisle reviewed the Conditional Rezoning and Special Use Approval application for the proposed Jax Kar Wash. He addressed its location, an existing pipeline that bisects the P (Vehicular Parking) area to the north, two different zoning designations and surrounding zonings. Mr. Carlisle defined the P zoning designation as a legacy zoning district with the intent to create a buffer. He addressed how the proposed rezoning relates to the Master Plan and Future Land Use designation of *The Smart Zone*.

Mr. Carlisle identified the conditions voluntarily offered by the applicant as cited in his report dated October 9, 2023. He noted the existing green space on the north side of the property that the applicant is offering to preserve is larger in size than the property zoned as Vehicular Parking. Mr. Carlisle stated if the Conditional Rezoning application is recommended for approval by the Planning Commission, a Conditional Rezoning Agreement would be drafted between the applicant and the City Attorney's office prior to consideration by the City Council.

Mr. Carlisle addressed site arrangement, access, circulation and landscaping. He asked the applicant to address (1) potential noise impact from the vacuums and car wash mechanical units, (2) consideration in removal or reducing the amount of stacking spaces, (3) the number of vacuum spaces, (4) site circulation as relates to customers entering to use the vacuums only, (5) screening of the east and west properties to meet Zoning Ordinance requirements, and (6) access from the medical office building located to the east.

Mr. Carlisle asked the Planning Commission to consider if the application meets the Rezoning Standards (Section 16.04C), Special Use Standards (Section 9.03), Site Plan Review Design Standards (Section 8.06) and the intent of the Future Land Use designation. He asked that any approval of the application be conditioned on the site plan items identified in his report dated October 9, 2023.

Discussion among Planning Commission and administration followed. Some comments related to:

- *The Smart Zone* designations within the City (one).
- Proposed use in relation to *The Smart Zone*, Master Plan and surrounding land uses.
- Proposed zoning district as relates to "spot" zoning.
- Quality of existing trees, landscaping.
- Development limitations on property to the north that is bisected by an existing pipeline.

Mr. Savidant acknowledged receipt of a formal Rezoning Protest Petition and written communication with signatures of West Oak subdivision residents, both of which address concerns with the proposed development.

Applicant Todd Gesund, Vice President, Director of Expansion of Jax Car Wash, introduced the development team present this evening.

- Jon Zimmerman, CEO, Jax Car Wash
- John Gaber, Partner, WWRP, Legal Representative
- Linden Ivezaj, P.E., Chief Operating Officer, Cunningham-Limp
- Jim Butler, Principal, PEA Group

Mr. Gesund presented a brief history of Jax Kar Wash. He addressed its excellent customer service and community involvement. Mr. Gesund said they met with surrounding neighbors to introduce the proposed development and address any concerns they might have. He said based on recent communication received by the City it is clear neighbors are not in favor of the project. Mr. Gesund addressed the application as relates to zoning, existing pipeline that bisects the Parking property, preservation of 42% of existing vegetation/landscape, distance of car wash from residential property line, and screening and view of the car wash from residential properties. He said they intend to never develop the landscaped Parking property.

Mr. Gesund also addressed the building design, site layout, traffic circulation, stacking of cars and its quiet operation. He said the hours of operation are 7 a.m. to 8 p.m. Monday through Saturday and 8 a.m. to 6 p.m. on Sunday. Mr. Gesund said the car wash is 80% membership-based, that approximately 3.8 cars are washed per month with an average of 475 cars daily. Photographs of the perspective view of the car wash from residents on Hartland were displayed.

Mr. Zimmerman presented a Jax Kar Wash noise study, citing decibel levels at certain points of operation. He said the car wash is extremely quiet because all the mechanical units and vacuum noise is contained inside the building.

Mr. Butler said appropriate screening for properties to the east and west would be added to comply with Zoning Ordinance requirements. He addressed site circulation and traffic pattern as relates to the car wash operation and use of vacuums. Mr. Butler said there is an existing cross access easement with the adjacent office building to the east and the applicant is amenable to future cross access easements if the City so desires.

Mr. Gaber guided Board members through a PowerPoint presentation to address how the application relates to the Conditional Rezoning Standards, Special Use Approval Standards, Master Plan and *The Smart Zone* Future Land Use designation. Some key points addressed by Mr. Gaber were:

- Voluntary conditions offered relating to use, site plan layout and preservation of landscaped area in perpetuity.
- IB zoning supports many of the uses described in *The Smart Zone* designation.
- Building design and building elements compatible with *The Smart Zone* designation.
- Surrounding zonings compatible with *The Smart Zone* designation.
- Intent of the rezoning request is to extend the IB zoning district.
- Car wash facility likely to service adjacent uses.
- Rezoning request has no negative impact on neighboring properties, public service or overall environment.
- Site design minimizes traffic impact.

(Ms. Dufrane exited the meeting at 8:23 p.m.; returned to the meeting at 8:26 p.m.)

There was discussion, some comments related to:

- Site circulation patterns; internal and external.
- Traffic pattern for customers who use vacuums only.
- Applicant's strategy to circumvent customers from using vacuums at no charge.
- Signage on site to direct traffic patterns for customers.
- Type of screening provided for properties located to the east and west.
- Landscape screening to the north; residents' view of car wash during winter months.
- Application in relation to the Master Plan and Future Land Use designation.
- Sound study presented by applicant.

PUBLIC HEARING OPENED

The following speakers voiced **opposition** to the proposed car wash.

- Moorthy Vivek, 1482 Hartland; addressed concerns that car wash is not a fit for *The Smart Zone* designation, negative effect on property value, privacy, light pollution, safety, noise and fumes, setting precedent of continuing IB zoning.
- Geeta Madurkar, 1494 Hartland; addressed concerns with negative impact on health and environment, property value, noise pollution, safety of school children, increased traffic.
- Bruce Knight, 1607 E. Big Beaver, Suite 210; addressed definition of *The Smart Zone* designation as relates to proposed development and compatibility with adjacent property, concerns with vehicular access to his property, traffic circulation and safety.
- Speaker did not identify himself by name or address but stated he lives behind proposed development; addressed concerns with noise, light pollution, increased traffic, difficulty in exiting subdivision and merging into ongoing traffic to make a turnaround on Big Beaver.
- Chandra Siddharthan, 1446 Hartland; addressed concerns with negative impact on quality of life and noise pollution. Mr. Siddharthan challenged applicant's sound study; shared from his phone traffic noise heard at his residence from Big Beaver traffic and street noise from a nearby Jax Kar Wash.
- Makarand Phadke, 1470 Hartland; addressed concerns for safety and security of children who play in area, noise pollution, increased traffic, circulation of traffic and compatibility with *The Smart Zone* designation.
- Venkat Aitharaju, 1410 Hartland; addressed concerns with noise, negative impact on property values, compatibility with *The Smart Zone* designation, encouraged high technology developments.
- Lokesh Jayachandran, 1530 Hartland; addressed concerns with noise pollution, exhaust fumes, lack of privacy; both he and his wife work from home.
- Nandkishor Dhilde, 1518 Hartland; addressed concerns with compatibility with *The Smart Zone* designation, increase in traffic especially during Spring season.
- Vivek Sandell, 1434 Hartland; addressed compatibility with *The Smart Zone* designation, increase in traffic, traffic circulation as relates to Big Beaver, encouraged diverse economic development like the Automation Alley designation.
- Daniel Bousho, 94 Hickory; addressed *The Smart Zone* designation in relation to proposed car wash, concerned with setting a precedent for future development.

PUBLIC HEARING CLOSED

Mr. Savidant stated the City Council has the final decision on a rezoning application. He said public comment can be heard again at the December 12, 2023 Regular Planning Commission meeting and at the City Council meeting.

Mr. Carlisle said a Public Hearing will be scheduled at the City Council meeting, noting the meeting most likely will be in the New Year.

Resolution # PC-2023-10-061

Moved by: Buechner
Seconded by: Fox

RESOLVED, To postpone Conditional Rezoning request (CR JPLN2023-001), located on the north side of Big Beaver, east of Rochester, to the December 12, 2023 meeting.

Yes: All present (7)
Absent: Faison, Krent

MOTION CARRIES

OTHER ITEMS

6. **PUBLIC COMMENT** – For Items on the Agenda

Vivek Sandell, 1434 Hartland; addressed administrative review process for the proposed conditional rezoning; said postponing item might be a convenient diversion on the part of the City.

Mr. Perakis said public comment this evening would be on the record and in the meeting minutes for future reference if residents are unable to attend the Planning Commission meeting in December.

Mr. Fox said the City Council gives serious consideration to the Planning Commission recommendations on proposed rezonings and encouraged residents to share their concerns with the City Council.

7. **PLANNING COMMISSION COMMENT**

There were general Planning Commission comments, some related to:

- Special Use Approval and zoning designations that permit car wash facilities.
- Consideration of City revenue is not within the purview of the Planning Commission.
- Legal implications relating to proposed development applications considered by the Planning Commission.
- Attendance at Michigan Association of Planning (MAP) conference.
- Chair Lambert and Commissioner Fox seeking Master Citizen Planner (MCP) credentials.

Mr. Savidant reported the Rezoning Request for Encore Development, East side of Todd, South of Big Beaver, from R-1C and P to Big Beaver zoning district was granted by City Council at their October 16, 2023 meeting.

8. ADJOURN

The Regular meeting of the Planning Commission adjourned at 9:01 p.m.

Respectfully submitted,

David Lambert, Chair

Kathy L. Czarnecki, Recording Secretary

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ITEM #5

DATE: November 8, 2023

TO: Planning Commission

FROM: R. Brent Savidant, Community Development Director

SUBJECT: SPECIAL USE APPROVAL AND PRELIMINARY SITE PLAN REVIEW (SU JPLN2023-0019) - Proposed Clean Express Car Wash, West side of John R, North of Fourteen Mile (PIN 88-20-35-401-001), Section 35, Zoned GB (General Business) District

The petitioner Mannik and Smith Group submitted the above referenced Special Use Approval and Preliminary Site Plan Approval application for a new full-service car wash within an underutilized parking lot at Oakland Mall.

Vehicle auto washes are permitted in the GB district, subject to special use approval.

The Planning Commission held a public hearing on this item on September 26, 2023 and postponed the item (minutes attached).

The attached report prepared by Carlisle/Wortman Associates, Inc. (CWA), the City's Planning Consultant, summarizes the application. CWA prepared the report with input from various City departments including Planning, Engineering, Public Works and Fire. City Management supports the findings of fact contained in the report and recommends approval of the project, as noted.

Attachments:

1. Maps
2. Report prepared by Carlisle/Wortman Associates, Inc.
3. Minutes from September 26, 2023 Regular meeting (excerpt)
4. Memo prepared by OHM Advisors dated October 18, 2023
5. Special use application

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PROPOSED RESOLUTION

SPECIAL USE APPROVAL AND PRELIMINARY SITE PLAN REVIEW (SU JPLN2023-0002) - Proposed Clean Express Car Wash, West side of John R, North of Fourteen Mile (PIN 88-20-35-401-001), Section 35, Zoned GB (General Business) District

Resolution # PC-2023-11-

Moved by:

Seconded by:

RESOLVED, That Special Use Approval and Preliminary Site Plan Approval for the proposed Clean Express Car Wash, West side of John R, North of Fourteen Mile (PIN 88-20-35-401-001), Section 35, Currently Zoned GB (General Business) District, be (granted, subject to the following conditions):

1. Add one (1) parking lot tree internal to the parking lot along the western parking area, and one (1) tree along the eastern parking lot area.
2. Stamp plan by licensed landscape architect.

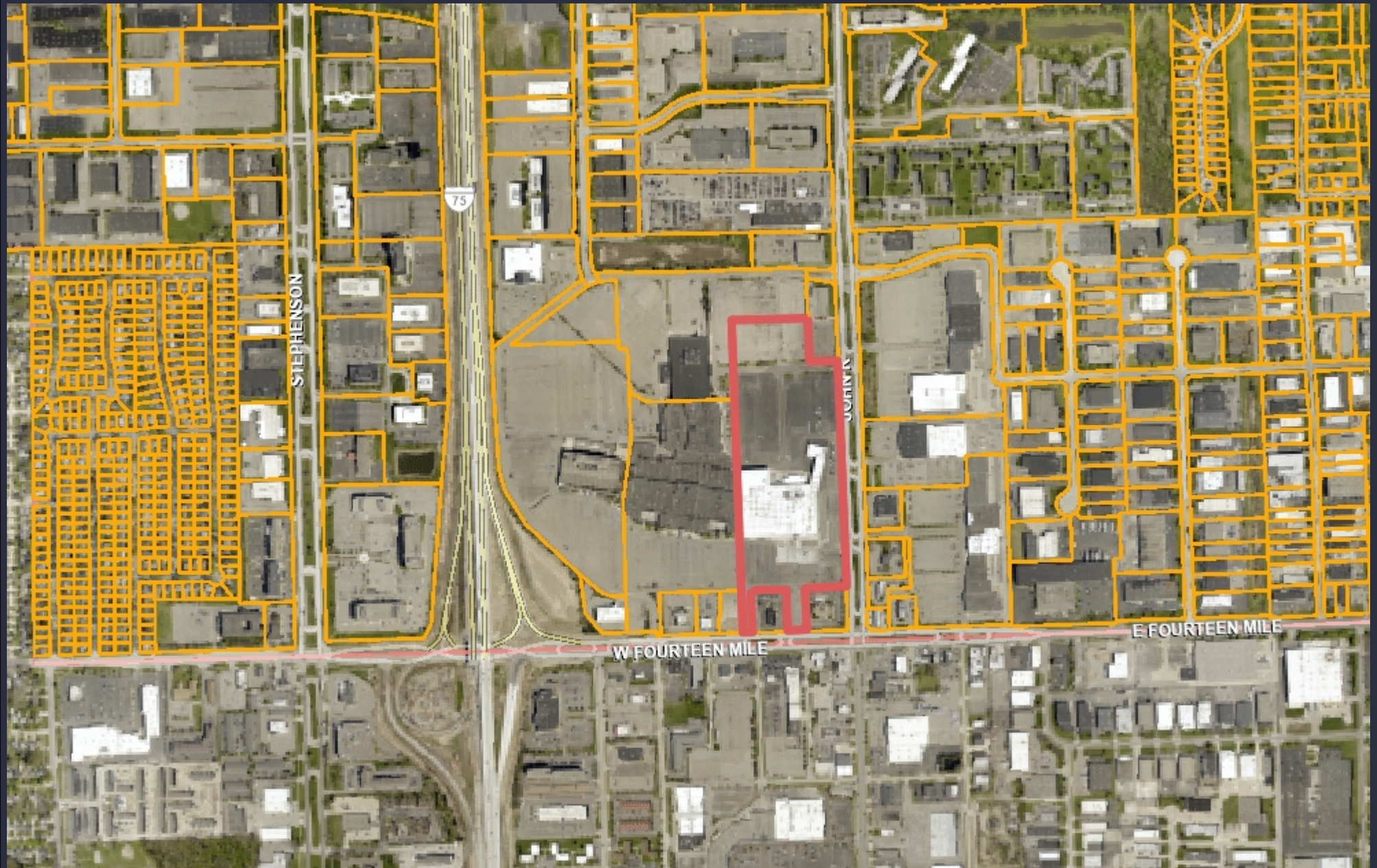
_____) or
 (denied, for the following reasons: _____) or
 (postponed, for the following reasons: _____)

Yes:
 No:
 Absent:

MOTION CARRIED / FAILED



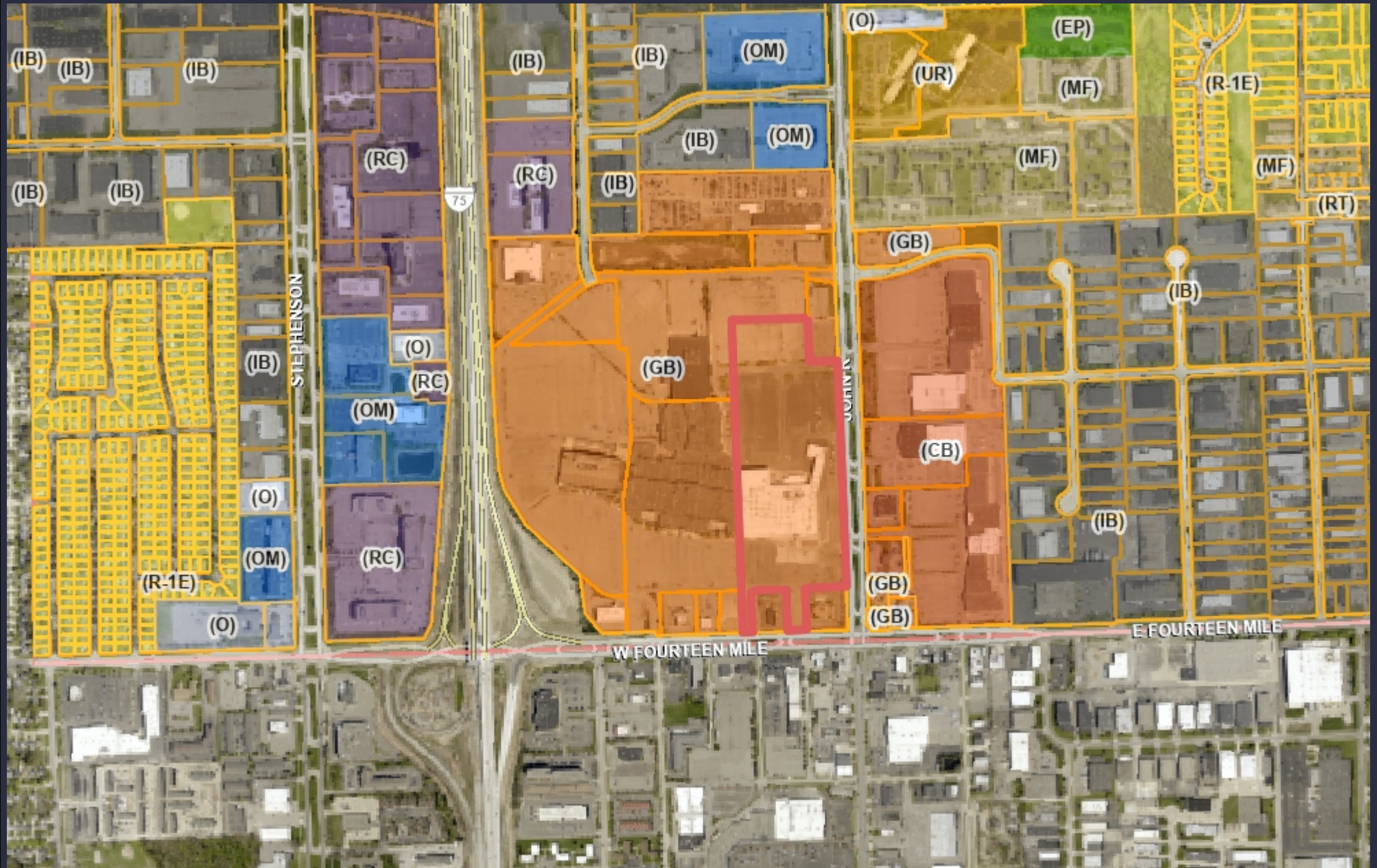
GIS Online



Note: The information provided by this application has been compiled from recorded deeds, plats, tax maps, surveys, and other public records and data. It is not a legally recorded map survey. Users of this data are hereby notified that the source information represented should be consulted for verification.



GIS Online



Note: The information provided by this application has been compiled from recorded deeds, plats, tax maps, surveys, and other public records and data. It is not a legally recorded map survey. Users of this data are hereby notified that the source information represented should be consulted for verification.



Carlisle | Wortman
ASSOCIATES, INC.

117 NORTH FIRST STREET SUITE 70 ANN ARBOR, MI 48104 734.662.2200 734.662.1935 FAX

Date: September 20, 2023
November 7, 2023

Preliminary Site Plan For City of Troy, Michigan

Applicant: Mannik and Smith Group

Project Name: Clean Express Car Wash

Location: Oakland Mall (outlot adjacent to John R)

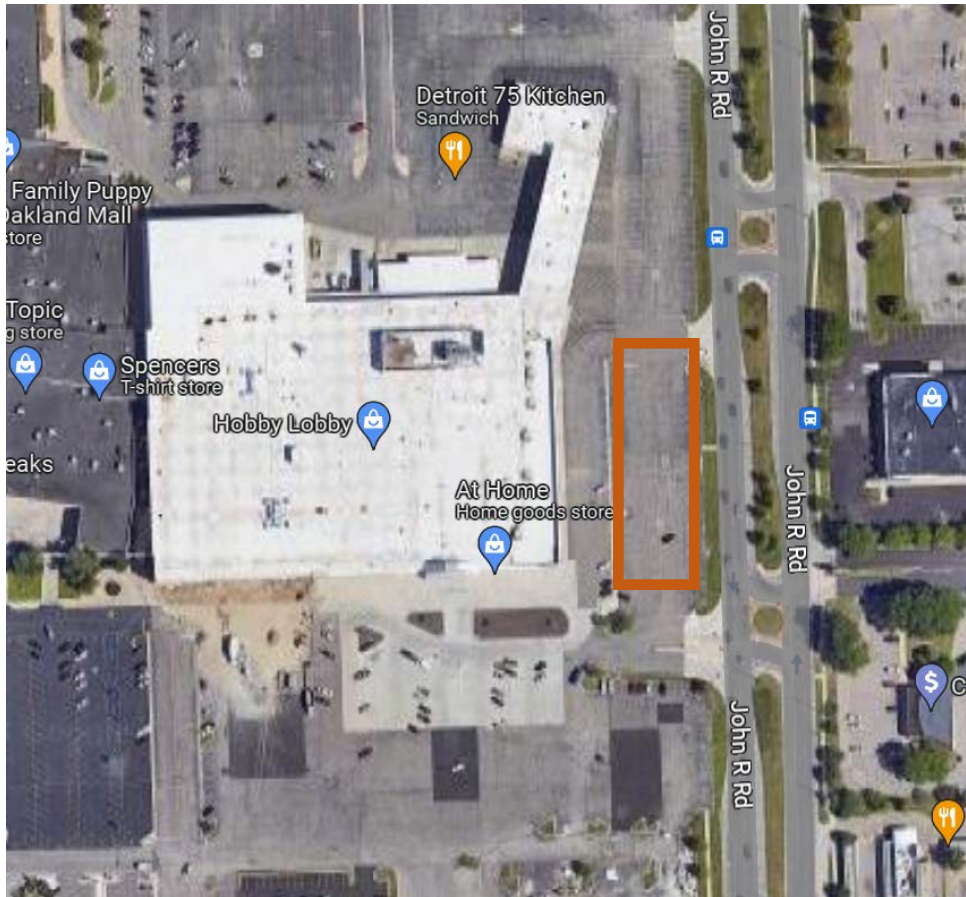
Zoning: GB, General Business

Action Requested: Preliminary Site Plan Review

SITE/PROJECT DESCRIPTION/CURRENT USE

The applicant has submitted an application for a new car wash to be located in an outlot of Oakland Mall. The proposed outlot is adjacent to John R and in front of the At Home store in the mall. The proposed carwash is 3,600 sq/ft and includes 25 vacuum stations. The parking and vacuum stations are located between the building and the At Home store. The area proposed for development is currently parking.

The site is zoned GB, General Business. A carwash is a special use in the GB district.



PREVIOUS PLANNING COMMISSION REVIEW

The Planning Commission last reviewed this project at their September 26, 2023 meeting. The item was postponed to allow OHM to review the traffic study.

Since the last meeting the applicant has made the following changes:

- Provided an 8 -foot sidewalk along John R Road
- Relocated the dumpster to internal to the site
- Provided a sidewalk across the north side of lot, to and from John R
- Added one (1) internal parking lot island along the western parking lot side

NATURAL FEATURES

The site has been graded and developed for use as a parking lot. There are no natural features in the area proposed for development.

Items to be addressed: None

SITE ARRANGEMENT

The proposed outlot is adjacent to John R and in front of the At Home store in the mall. The proposed carwash is 3,600 sq/ft and includes 25 vacuum stations. The parking and vacuum stations are located between the building and parking for the At Home store. The area proposed for development is currently parking.

Items to be addressed: None

AREA, WIDTH, HEIGHT, SETBACKS

Section 4.14 establishes the dimensional requirements for GB, General Business District. The requirements of GB Zoning District and the proposed dimensions are shown in the following table.

	Required	Provided	Compliance
Front (John R)	10-feet	10.55-feet	Complies
Side	20-feet /40-feet combined	Over 20-feet and 40-feet combined	Complies
Rear	30-foot minimum setback	Over 30-feet	Complies
Building Height	3-stories, 40 feet in height.	3-stories, 40 feet in height	Complies
Parking Location	NO more than 50% can be in front yard	No more than 50% of parking is located in front yard	Complies

Items to be addressed: None

PARKING

Section 13.06.G of the Zoning Ordinance requires:

	Required	Provided	Compliance
Car Wash	2 spaces, plus 12 stacking spaces per bay for a semi- or fully-automatic wash, 1 stacking space per bay for a self-serve wash, plus 1 space per 1 employee on the largest typical shift	25 vacuum spaces and 3 non-vacuum spaces. Over 12 stacking spaces.	Complies

Barrier Free	1	1	Complies
Bicycle Parking	2	2	Complies
Loading	0	0	

Items to be Addressed: None

TRAFFIC, SITE ACCESS, and CIRCULATION

Internally, the site is curbed to separate the use from general mall parking and circulation. OHM has reviewed the traffic study prepared by Mannik Smith Group dated October 13, 2023. OHM recommends approval of the traffic impact assessment and site plan. See OHMs October 18, 2023 memo for more details.

OHM, the City Engineer, and the City Fire Marshall have reviewed site circulation and does not have any objections.

Items to be addressed: None

LANDSCAPING

A landscaping plan has been provided on Sheet L101. The following table discusses the development’s compliance with the landscape requirements set forth in Section 13.02.

	Required:	Provided:	Compliance:
Greenbelt Planting			
John R: 1 tree every 30 feet	$328 / 30 = 11$	18 trees	Complies
Parking Lot Landscaping:			
1 tree per 8 parking spaces	$29 / 10 = 4$ trees	4 trees in perimeter of parking area.	Complies with Planning Commission consideration
Overall			
Site landscaping: A minimum of twenty percent (20%) of the site area shall be comprised of landscape material. Up to twenty-five percent (25%) of the required landscape area may be brink, stone, pavers, or other public plaza elements, but shall not include any parking area or required sidewalks.	$20\% = 8,540$ sq/ft	Over $20\% = 10,332$ sq/ft	Complies

We note that there is a long stretch of parking (with vacuums) that does not have any parking lot landscaping. We had recommended that the applicant provide two (2) parking lot trees internal to the parking lot along the western parking area, and one (1) tree along the eastern parking lot area. Parking lot trees in the western parking area will also breakup and help distinguish and define the property and use boundaries of the carwash. The applicant added one (1) parking lot tree along the western parking lot area. Landscape Plan was not stamped by a licensed landscape architect.

Items to be Addressed: 1). Add one (1) parking lot tree internal to the parking lot along the western parking area, and one (1) tree along the eastern parking lot area; and 2). Stamp plan by licensed landscape architect.

PHOTOMETRICS

The applicant proposes three (3) parking lot pole lights and six (6) building lights. Photometric and fixtures comply with ordinance requirements.

Items to be Addressed: None

FLOOR PLAN AND ELEVATIONS

Floor plans and elevations have been provided. The primary material is brick. A rendering shows a color scheme of white brick, natural brick, and green vacuums. The proposed canopy over the pay area is red.

Items to be Addressed: None

SPECIAL USE STANDARDS

For any special use, according to Section 9.02.D, the Planning Commission shall “...review the request, supplementary materials either in support or opposition thereto, as well as the Planning Department’s report, at a Public Hearing established for that purpose, and shall either grant or deny the request, table action on the request, or grant the request subject to specific conditions.”

Section 9.03 states that before approving any requests for Special Use Approval, the Planning Commission shall consider:

1. *Compatibility with Adjacent Uses.*
2. *Compatibility with the Master Plan.*
3. *Traffic Impact.*
4. *Impact on Public Services.*
5. *Compliance with Zoning Ordinance Standards.*
6. *Impact on the Overall Environment.*
7. *Special Use Approval Specific Requirements.*

SITE PLAN REVIEW STANDARDS

The Site Plan review standards provide the Planning Commission with direction when reviewing the proposed site plan and design features of this development.

Section 8.06 outlines Site Plan Review Design Standards.

Section 8.06 outlines Site Plan Review Design Standards.

1. *Development shall ensure compatibility to existing commercial districts and provide a transition between land uses.*
 - a. *Building design shall enhance the character of the surrounding area in relation to building and parking placement, landscape and streetscape features, and architectural design.*
 - b. *Street fronts shall provide a variety of architectural expression that is appropriate in its context and prevents monotony.*
 - c. *Building design shall achieve a compatible transition between areas with different height, massing, scale, and architectural style.*

2. *Development shall incorporate the recognized best architectural building design practices.*
 - a. *Foster a lasting impact on the community through the provision of high quality design, construction, and detailing.*
 - b. *Provide high quality, durable materials, such as but not limited to stone, brick, glass, and metal. E.I.F.S. or material equivalent shall only be used as an accent material.*
 - c. *Develop buildings with creativity that includes balanced compositions and forms.*
 - d. *Design roofs that are appropriate to the architectural style of the building and create an appropriate visual exterior mass of the building given the context of the site.*
 - e. *For commercial buildings, incorporate clearly defined, highly visible customer entrances using features such as canopies, porticos, arcades, arches, wing walls, ground plane elements, and/or landscape planters.*
 - f. *Include community amenities that add value to the development such as patio/seating areas, water features, art work or sculpture, clock towers, pedestrian plazas with park benches or other features located in areas accessible to the public.*

3. *Enhance the character, environment and safety for pedestrians and motorists.*
 - a. *Provide elements that define the street and the pedestrian realm.*
 - b. *Create a connection between the public right of way and ground floor activities.*
 - c. *Create a safe environment by employing design features to reduce vehicular and pedestrian conflict, while not sacrificing design excellence.*
 - d. *Enhance the pedestrian realm by framing the sidewalk area with trees, awnings, and other features.*

- e. *Improve safety for pedestrians through site design measures.*

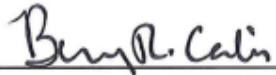
SUMMARY

The Planning Commission is asked to consider if the plan meets the special use and site plan design standards.

If the Planning Commission approves the special use and site plan, the following site plan items should be addressed by the applicant:

1. Add one (1) parking lot tree internal to the parking lot along the western parking area, and one (1) tree along the eastern parking lot area.
2. Stamp plan by licensed landscape architect.

Sincerely,



CARLISLE/WORTMAN ASSOC., INC.
Benjamin R. Carlisle, LEED AP, AICP

SPECIAL USE APPROVAL

5. **PUBLIC HEARING – SPECIAL USE APPROVAL AND PRELIMINARY SITE PLAN REVIEW (SU JPLN2023-0019)** – Proposed Clean Express Car Wash, West side of John R, North of Fourteen Mile (PIN 88-20-35-401-001), Section 35, Zoned GB (General Business) District

Mr. Carlisle reviewed the Clean Express Car Wash Special Use and Preliminary Site Plan application. He addressed the car wash location at the Oakland Mall, site circulation, proposed landscaping, architectural features, building size and number of vacuums. He reported the application is permitted as a Special Use in the GB zoning district and that it meets parking requirements. Mr. Carlisle noted the rendering provided in the agenda packet is not site specific but a general rendering of the proposed development.

In summary, Mr. Carlisle asked the Board to consider any public testimony during the Public Hearing and to consider in its deliberations site circulation, especially around the location of the parking lot entrance, building materials and color scheme, and if the plan meets the Special Use and Site Plan Review Design Standards as set forth in Sections 9.02.D and 8.06, respectively. Mr. Carlisle stated any approval of the application should be conditioned on site plan items identified in his report dated September 20, 2023.

Mr. Savidant announced the applicant provided a site-specific rendering after preparation of the Planning Consultant report. He said the rendering was emailed to Board members prior to tonight's meeting and is available to view on the Board's laptop and on the presentation screen. Mr. Savidant said a traffic study has been provided by the applicant but due to time constraints not yet reviewed by the City Traffic Engineer OHM.

Jacob Rilett of Mannik Smith Group, Consultant Dennis Miller and Oakland Mall owner Mario Kiezi were present.

Mr. Rilett provided a brief description of the modernized express car wash that has ninety locations throughout the Midwest. He highlighted its quick service, advanced water features and conservation and subscription membership. Mr. Rilett addressed stormwater management in relation to the overall mall property and topography.

A revised site plan was circulated among Board members that addressed site plan issues identified in the Planning Consultant report.

There was discussion, some comments related to:

- Ownership of the Oakland Mall property.
- Stormwater management in relation to the existing topography and mall property.
- Conservation of water usage.
- Construction of sidewalk connections per Engineering review.
- Relocation of dumpster.
- Bicycle parking.
- Landscaping; additional parking lot trees.
- Number of vacuums.

- Site circulation, as relates to entry and exit and Oakland Mall traffic.
- Stacking of vehicles.
- Employee parking.
- Relationship of application to Master Plan.
- Future development of mall property.

Mr. Miller said the car wash would be ready to facilitate high volumes during weekends and adverse weather conditions and signage would be provided to direct traffic. He addressed the in-and-out quick express service, selection of services and price range of membership. Mr. Miller said the facility would be community focused, offering fundraisers.

Mr. Kiezi gave a brief history of the Oakland Mall, its anchor stores and recently acquired stores. He said a Master Plan is in process to develop the entire mall property to incorporate mixed uses and change the overall view of the property. Mr. Kiezi addressed the mall's advantageous location to the I-75 frontage and surrounding communities. He addressed ownership of the mall property, marketability of the site, ongoing site improvements and focus on acquiring quality tenants.

PUBLIC HEARING OPENED

There was no one present who wished to speak.

PUBLIC HEARING CLOSED

There was a brief discussion on the revised site plan circulated by the applicant that addressed site items identified in the Planning Consultant report.

Mr. Savidant and Ms. Dufrane emphasized the importance of OHM reviewing the traffic study prior to consideration of site plan approval.

Resolution # PC-2023-09-057

Moved by: Buechner

Seconded by: Fox

RESOLVED, To postpone action on Special Use Approval and Preliminary Site Plan Approval to allow review of the traffic study by the City Traffic Engineer.

Yes: All present (9)

MOTION CARRIED



memorandum

Date: October 18, 2023

To: Brent Savidant

From: Stephan Maxe, PE & Lauren Hull, EIT

Re: Proposed Express Wash Development
Traffic Impact Assessment Review

We have reviewed the traffic impact assessment for the proposed Express Wash development in Troy, Michigan. The report was prepared by Mannik Smith Group and is dated October 13, 2023. The site plan was prepared by Mannik Smith Group and is dated July 18, 2023.

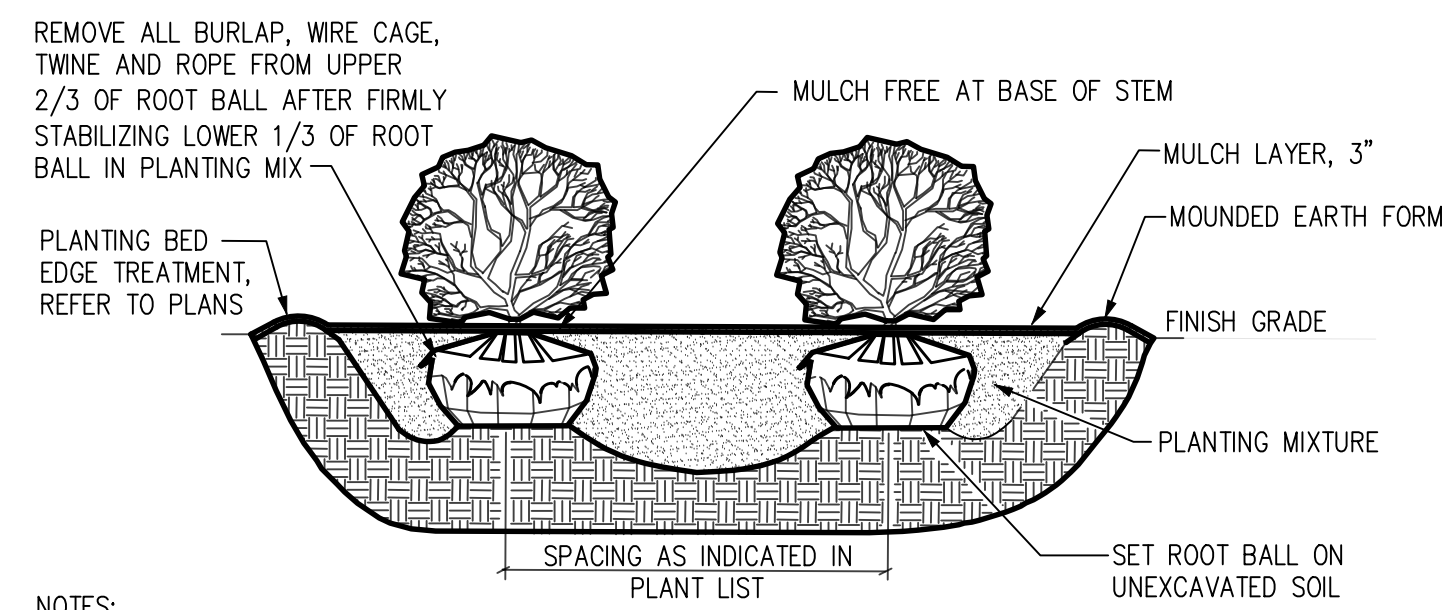
The applicant submitted hourly peak station count data from five similar car wash facilities in cities in Ohio. These cities are Hilliard, Columbus, Heath, Gahanna, and Dublin. Data shows an estimated 56 site generated trips during the weekday AM peak hour and 75 site generated trips during the weekday PM peak hour. Additionally, the data shows an estimated 77 site generated trips during the weekend peak hour. OHM does not object to use of this data; however, we note that this data assumes that the month of April, when the provided counts are from, reflect year-round conditions and that employees and deliveries were not included in these counts.

OHM recommends approval of the traffic impact assessment and site plan at this time.

LANDSCAPE PLANTING NOTES

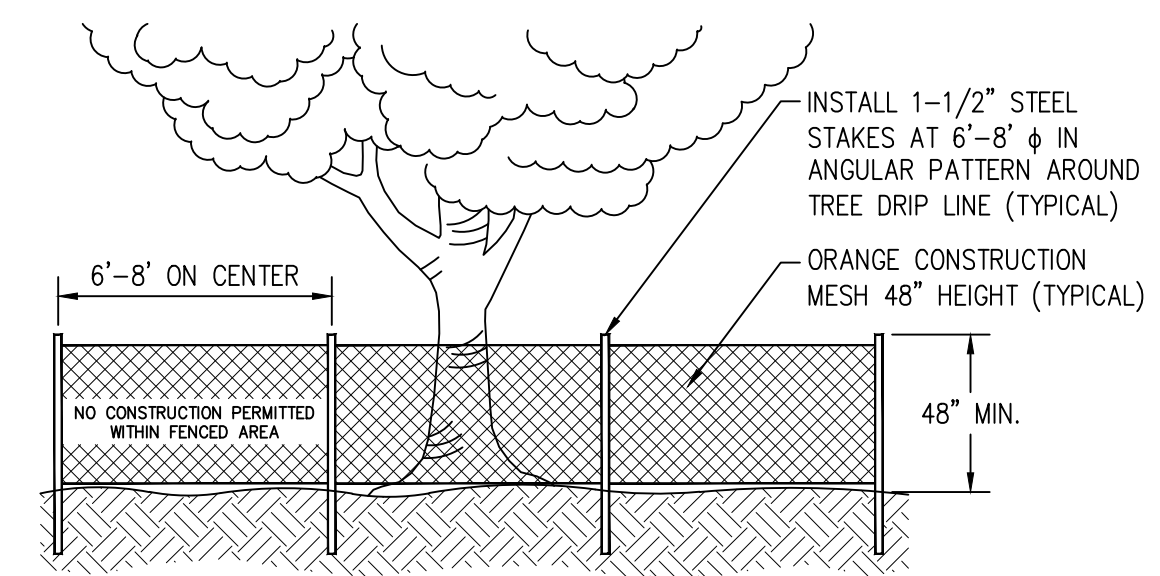
1. THE CONTRACTOR SHALL PROVIDE A ONE YEAR WARRANTY ON ALL PLANTS AND VEGETATION PROPOSED ON THE LANDSCAPING PLAN. ANY TREES, SHRUBS, GROUND COVER OR OTHER VEGETATION PLANTED AS PART OF THIS PROJECT THAT DO NOT SURVIVE ONE YEAR FROM PLANTING SHALL BE REPLACED AT THE EXPENSE OF THE CONTRACTOR.
2. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING AND COORDINATING WITH ALL PERTINENT UTILITY COMPANIES THREE WORKING DAYS IN ADVANCE OF ANY DIGGING. THE CONTRACTOR SHALL ASSUME SOLE RESPONSIBILITY FOR ANY COST INCURRED DUE TO DAMAGE OF ANY UTILITIES.
3. REFER TO THE LANDSCAPE PLAN FOR ADDITIONAL NOTES. LANDSCAPE IMPROVEMENTS SHALL CONFORM TO THE LATEST EDITION OF MDOT CONSTRUCTION SPECIFICATIONS (2020 MDOT STANDARD SPECIFICATIONS FOR CONSTRUCTION).
4. ALL PLANTING MATERIALS: SHALL BE PLANTED PER MDOT SPECIFICATIONS. EXISTING TREES TO REMAIN SHALL BE PRUNED TO REMOVE BROKEN, LOW HANGING AND OTHER UNDESIRABLE GROWTH TO ENSURE HEALTHY AND SYMMETRICAL NEW GROWTH.
5. PLANTING BEDS: ONE MONTH BEFORE CULTIVATION AND AFTER DAYTIME TEMPERATURES HAVE WARMED TO 60° CONSISTENTLY, TREAT ALL PLANTING BEDS THAT ARE TO BE DEVELOPED IN AREAS OF EXISTING TURF WITH PRE-EMERGENT AND POST-EMERGENT TYPE HERBICIDES. USE A STATE-LICENSED PESTICIDE APPLICATOR TO APPLY THE HERBICIDE. REPEAT HERBICIDE APPLICATION TWO WEEKS LATER AND UNTIL ALL HERBACEOUS MATERIALS HAVE BEEN KILLED. BEFORE PLANTING, TOP DRESS ALL PLANTING BEDS WITH A MINIMUM OF 6 INCHES OF BACKFILL MIX, THEN CULTIVATE PLANTING AREA TO A DEPTH OF 6 INCHES USING A PLOW, DISC, OR ROTO-TILLER.
6. BACKFILL MIX: FOR ALL PLANTINGS, USE BACKFILL MIX CONSISTING OF THE FOLLOWING:
 - A. ONE PART EXCAVATED SOIL.
 - B. ONE PART TOPSOIL.
 - C. ONE PART EPA RATED CLASS IV COMPOST.
 - D. A SLOW RELEASE COMMERCIAL FERTILIZER (0-20-20 OR EQUAL) ADDED AT A RATE OF 5 POUNDS PER CUBIC YARD TO THE BACKFILL MIX.
 - E. IF SOIL AREAS ARE OF HIGH PH (GREATER THAN 6.5), APPLY 1.25 POUNDS OF ELEMENTAL SULFUR PER CUBIC YARD OF BACKFILL MIX.NOTE: CONTRACTOR SHALL SUPPLY A DETAILED SOIL ANALYSIS PRIOR TO ALL PLANT BED PREPARATION. ANALYSIS SHALL INDICATE SOIL PH, TEXTURE, MAJOR NUTRIENTS, SALTS, ETC. SOIL ANALYSIS SHALL BE FROM A REPUTABLE, INDEPENDENT LAB. SOIL AMENDMENTS SHALL BE INCORPORATED INTO BACKFILL/PLANT MIX AS RECOMMENDED BY THE INDEPENDENT LAB.
7. MULCH: SMOOTH AND SHAPE THE BACKFILL MIX TO FORM A SHALLOW BASIN SLIGHTLY LARGER THAN THE PLANTING HOLE. MULCH ALL PLANTING AREAS WITH A LAYER OF FINELY SHREDDED HARDWOOD BARK OF UNIFORM TEXTURE & SIZE. PLANTS GROUPED IN MASSES SHALL HAVE THE ENTIRE CONTIGUOUS PLANTING BED OR ISLAND MULCHED. USE SHREDDED HARDWOOD BARK ACED MIN. ONE YEAR. RAKE AND SMOOTH THE ENTIRE AREA OF THE PLANTING BEDS, MULCH TO A DEPTH OF 3 INCHES. AFTER MULCHING AND BEFORE WATERING, ADD A SLOW RELEASE COMMERCIAL FERTILIZER (12-12-12 OR EQUAL), IN GRANULAR FORM, TO THE TOP OF THE MULCH AT A RATE OF 5 POUNDS PER 1000 SQUARE FEET. DO NOT ALLOW FERTILIZER TO CONTACT THE STEMS, BRANCHES, ROOTS OR LEAVES.
8. PERIOD OF ESTABLISHMENT: BEFORE FINAL INSPECTION, PLACE ALL PLANTS, SEED ALL LAWNS, AND CARE FOR THEM FOR A PERIOD OF ESTABLISHMENT. THE PERIOD OF ESTABLISHMENT BEGINS IMMEDIATELY UPON COMPLETION OF THE PLANTING OPERATIONS AND CONTINUES UNTIL OCTOBER 1. THE MINIMUM PERIOD OF ESTABLISHMENT IS ONE GROWING SEASON, JUNE 1 THROUGH OCTOBER 1. DURING THE PERIOD OF ESTABLISHMENT, FOLLOW STANDARD HORTICULTURAL PRACTICES TO ENSURE THE VIOR AND GROWTH OF THE TRANSPLANTED MATERIAL. WATER, REMULCH, RESTAKE, GUY, AND CULTIVATE AS NECESSARY. PERFORM AT LEAST TWO WEEDING AND MOWING PROGRAMS (AROUND TREES, GUY STAKES, SHRUBS, AND BED EDGES) OF SUCH INTENSITY AS TO COMPLETELY RID THE PLANTED AND MULCHED AREAS OF WEEDS AND GRASSES. BEGIN THE FIRST PROGRAM ON OR ABOUT JUNE 15 AND THE SECOND APPROXIMATELY 8 WEEKS LATER, ON OR ABOUT AUGUST 15, THE ENGINEER WILL INSPECT THE PLANTING AND SUPPLY THE CONTRACTOR WITH A LIST OF MISSING AND DEAD PLANTS AND THOSE THAT HAVE DIED BACK BEYOND NORMAL PRUNING LINES. REPLANT AS REQUIRED ACCORDING TO THE SPECIFICATIONS OF THE ORIGINAL MATERIAL. REPLACEMENT PLANTS ARE SUBJECT TO A NEW PERIOD OF ESTABLISHMENT. IMMEDIATELY REPLACE PLANTS PLANTED INITIALLY IN THE FALL THAT HAVE DIED BEFORE THE SPRING PLANTING SEASON. CARE FOR THE REPLACEMENT PLANTS DURING THE NEW ESTABLISHMENT PERIOD.
9. RESTORATION OF DISTURBED AREAS FOR NEW LAWN:
 - ALL DISTURBED AREAS NOT COVERED BY BUILDING, PAVEMENT OR LANDSCAPE PLANTING BEDS SHALL BE PREPARED FOR GRASS SEED AND SEEDED. LOOSEN RITS AND WORK THE SOIL AREAS TO A MINIMUM OF 6" DEEP PRIOR TO FINE GRADING AND SEEDING WORK. AREAS TO RECEIVE GRASS SEED SHALL HAVE A MIN. 4" TOPSOIL PLACED, SEEDED AND A STRAW/MULCH BLANKET COVER PLACED OVER THE SEEDED AREAS PER MDOT SPECIFICATIONS. FERTILIZE WITH ONE POUND OF ACTUAL NITROGEN PER 1000 SQUARE FEET WITH A SLOW RELEASE COMMERCIAL STARTER FERTILIZER (LESCO 18-24-12 OR EQUAL).
10. LANDSCAPE TREES, SHRUBS AND PERENNIAL WATERING:
 - THE CONTRACTOR SHALL BE RESPONSIBLE FOR FURNISHING, DELIVERING, APPLYING, MEASURING AND SCHEDULING A SUFFICIENT AMOUNT OF WATER NECESSARY TO KEEP EACH PLANT IN A HEALTHY GROWING CONDITION THROUGHOUT THE PERIOD OF ESTABLISHMENT. THE CONTRACTOR SHALL APPLY 1" OF WATER PER WEEK TO ALL NEW PLANTS. THE CONTRACTOR SHALL INSTALL & MAINTAIN SUPPLEMENTAL DRIP WATERING TREE BAGS (SUCH AS 20 GALLON TREE GATOR WATER BAG) TO PROVIDE ADEQUATE, SLOW RELEASE OF WATER. WATER BAGS SHALL BE REMOVED AT THE END OF THE SECOND GROWING SEASON.
11. TURF GROUNDCOVER (SODDING, SEEDING AND SEED MULCHING):
 - ALL SEEDING INSTALLATION SHALL CONFORM TO MDOT SPECIFICATIONS AND NOTE 9 ABOVE. SEED AT 5 LBS/1000 SF (220 LB/AC) WITH THE FOLLOWING SEED MIXTURE (CSI GEOTURF SUN & SHADE MIX):

KENTUCKY BLUEGRASS	20%
HARD FESCUE	20%
CHEWING FESCUE	20%
PERENNIAL RYEGRASS	40%



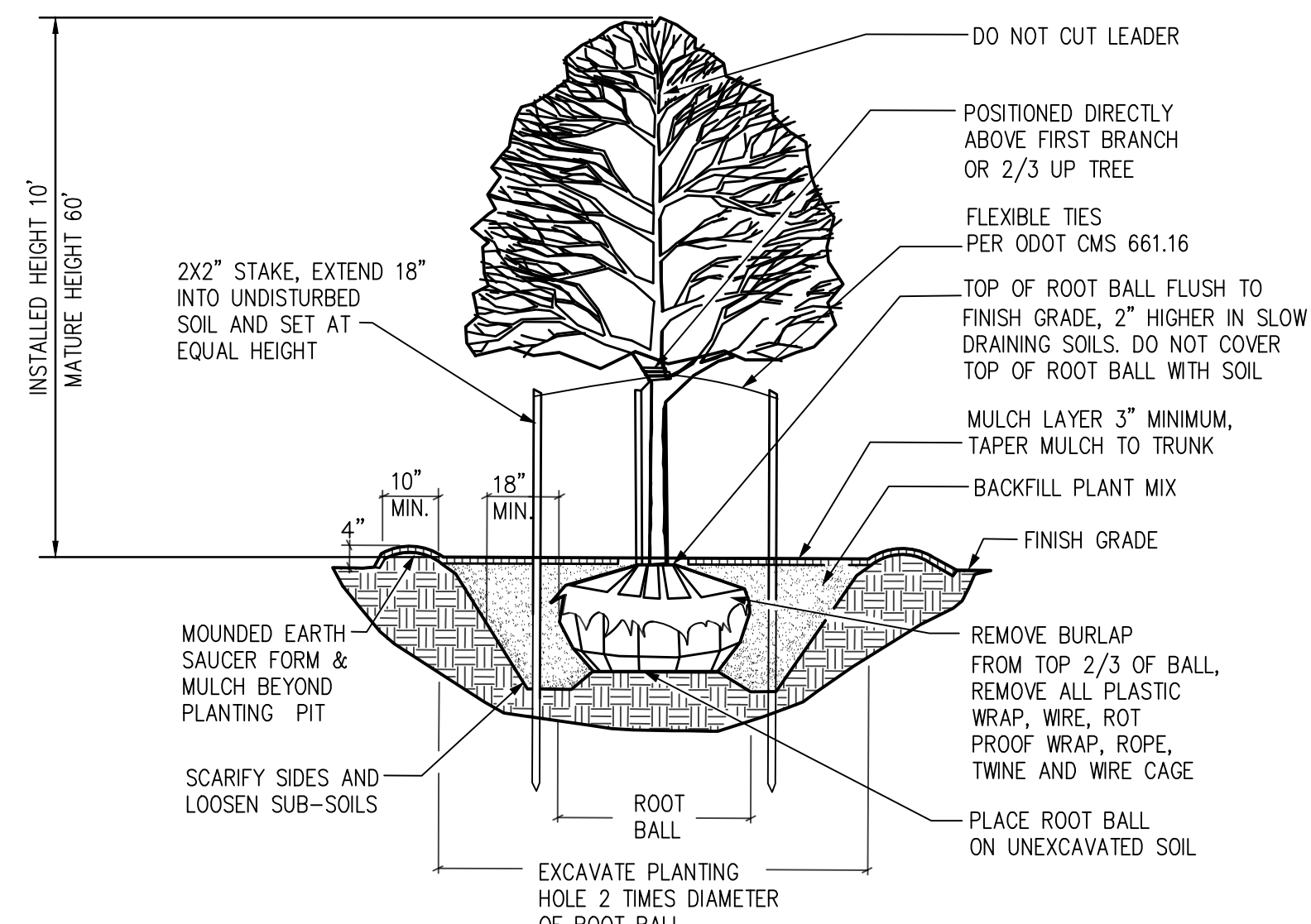
NOTES:
ALL SHRUBS PLANTED IN ROWS OR MASSES SHALL BE MATCHED IN SIZE AND FORM.
SHRUBS SHALL BEAR SAME RELATION TO FINISH GRADE AS THEY BORE TO EXISTING GRADE IN THE PREVIOUSLY PLANTED CONDITION.

MASS SHRUB PLANTING DETAIL (B&B OR CONTAINER)
NO SCALE

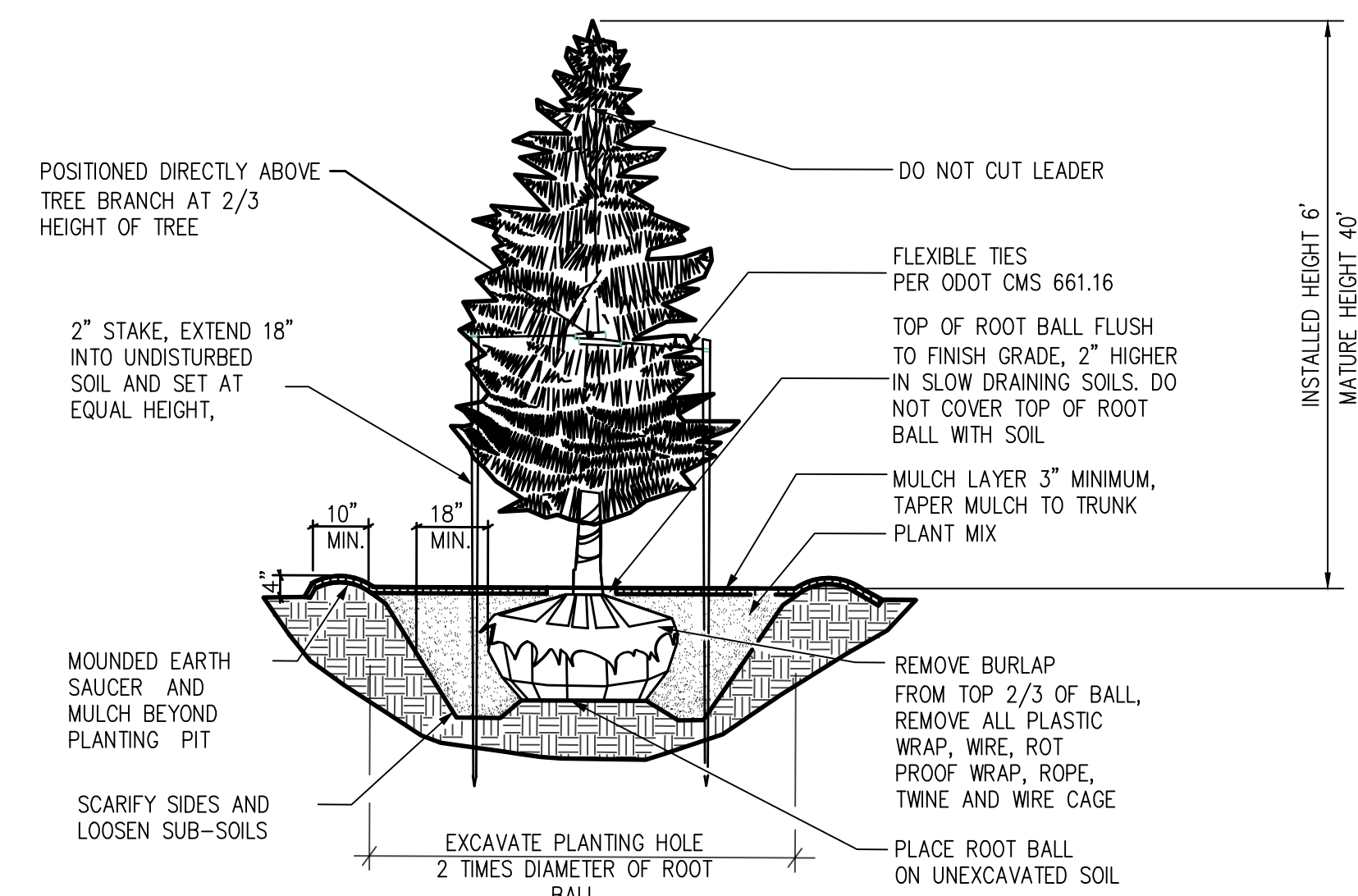


TREE PROTECTION DETAIL
NOT TO SCALE

- TREE PROTECTION NOTES
1. TREE PROTECTION MEASURES SHALL BE INSTALLED PER THIS PLAN SHEET AT THE CRITICAL ROOT ZONE OF TREES TO BE PRESERVED PRIOR TO THE START OF ANY CLEARING, GRADING OR OTHER CONSTRUCTION ACTIVITY.
 2. NO STORAGE OF ANY KIND IS PERMITTED WITHIN THE TREE PROTECTION ZONE. ANY ROOTS EXPOSED BY CONSTRUCTION ACTIVITY SHALL BE PRUNED FLUSH WITH THE SOIL AND BACKFILLED WITH TOPSOIL WITHIN THE SAME DAY.
 3. ALL VEGETATION THAT IS NOT DESIGNATED ON THE PLANS TO BE REMOVED SHALL BE PROTECTED FROM DAMAGE. TREES THAT ARE DAMAGED BY THE CONTRACTOR'S OPERATIONS SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE.



DECIDUOUS TREE PLANTING DETAIL
NO SCALE



EVERGREEN TREE PLANTING DETAIL
NO SCALE

NO. DATE BY DESCRIPTION			
NOT TO BE USED AS CONSTRUCTION DRAWINGS			
		PROJECT NO. 4012300176	
		DRAWN BY: JAR	
CHECKED BY: KJV			
PROJECT DATE: 07/18/2023			
607 SHELBY ST. DEPT. OF PUBLIC WORKS TEL: 313.981.9600 FAX: 313.981.9609			
PREPARED FOR: EXPRESS Wash Concepts 13375 NATIONAL ROAD SW ETAH, OHIO 43068			
SITE PLAN FOR CLEAN EXPRESS CAR WASH W 14 MILE ROAD TROY, MI 48063			
LANDSCAPE NOTES & DETAILS			
L101			

MICHIGAN
Utility Notification
CENTER
811
811 or
1-800-482-7171
Know What's Below. Call Before You Dig.

W:\p\2023\4012300176\018\64012300176\001\LANDSCAPE PLAN.dwg, 09/18/2023 2:27 PM by: jjar, jobno: 0102003281N

ITEM #6

DATE: November 8, 2023

TO: Planning Commission

FROM: R. Brent Savidant, Community Development Director

SUBJECT: PRELIMINARY SITE PLAN REVIEW (File Number SP JPLN2023-0026) – Proposed Controlled Exit onto Colebrook from Parking Lot, Northwest corner of Rochester and Colebrook (3635 Rochester; PIN 88-20-22-276-051, -052 & -053), Section 22, Currently Zoned GB (General Business) and R-1C (One Family Residential) District

The petitioner Nima Group LLC submitted the above referenced Preliminary Site Plan application for an additional exit only onto Colebrook for the ADAM Community Center (place of worship). The Planning Commission is authorized to approve changes in vehicular access.

The attached report prepared by Carlisle/Wortman Associates, Inc. (CWA), the City's Planning Consultant, summarizes the project. CWA prepared the report with input from various City departments including Planning, Engineering, Public Works and Fire. City Management supports the findings of fact contained in the report and the recommendations included therein.

Attachments:

1. Maps
2. Report prepared by Carlisle/Wortman Associates, Inc.
3. Memo from OHM Advisors dated October 19, 2023.

G:\SITE PLANS\SP_JPLN2023-0026_CONTROLLED EXIT FROM PARKING LOT\PC Memo 2023 11 14.docx

PROPOSED RESOLUTION

PRELIMINARY SITE PLAN REVIEW (File Number SP JPLN2023-0026) – Proposed Controlled Exit onto Colebrook from Parking Lot, Northwest corner of Rochester and Colebrook (3635 Rochester; PIN 88-20-22-276-051, -052 & -053), Section 22, Currently Zoned GB (General Business) and R-1C (One Family Residential) District

Resolution # PC-2023-11-

Moved by:

Support by:

RESOLVED, That Preliminary Site Plan Approval, pursuant to Article 8 of the Zoning Ordinance, as requested for the proposed Controlled Exit onto Colebrook from Parking Lot, Northwest corner of Rochester and Colebrook (3635 Rochester; PIN 88-20-22-276-051, -052 & -053), Section 22, Currently Zoned GB (General Business) and R-1C (One Family Residential) District, be (granted, subject to the following conditions):

_____) or

(denied, for the following reasons: _____) or

(postponed, for the following reasons: _____)

Yes:

No:

Absent:

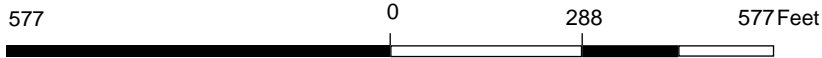
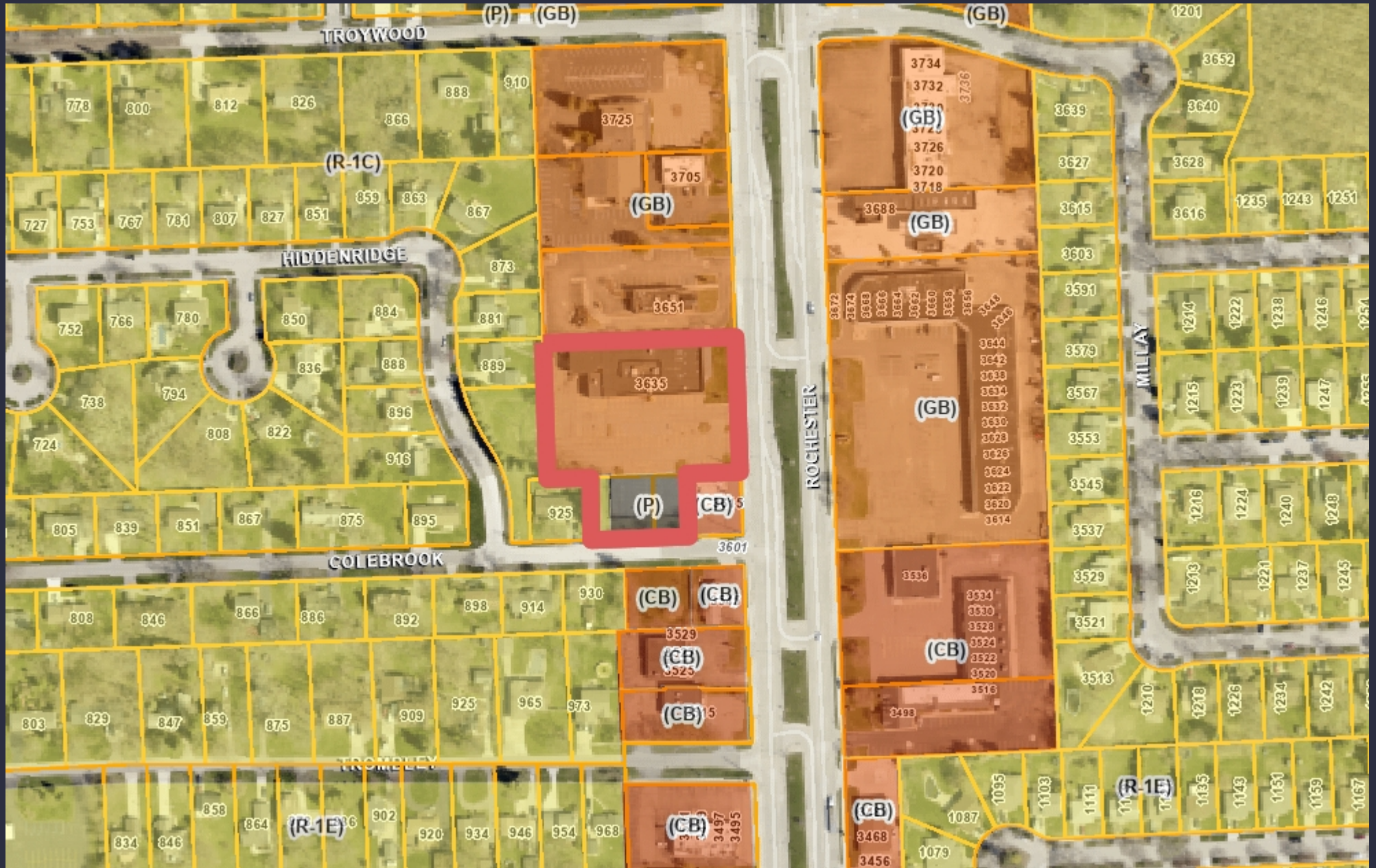
MOTION CARRIED



577 0 288 577Feet



Note: The information provided by this application has been compiled from recorded deeds, plats, tax maps, surveys, and other public records and data. It is not a legally recorded map survey. Users of this data are hereby notified that the source information represented should be consulted for verification.



Note: The information provided by this application has been compiled from recorded deeds, plats, tax maps, surveys, and other public records and data. It is not a legally recorded map survey. Users of this data are hereby notified that the source information represented should be consulted for verification.



Carlisle | Wortman
ASSOCIATES, INC.

117 NORTH FIRST STREET SUITE 70 ANN ARBOR, MI 48104 734.662.2200 734.662.1935 FAX

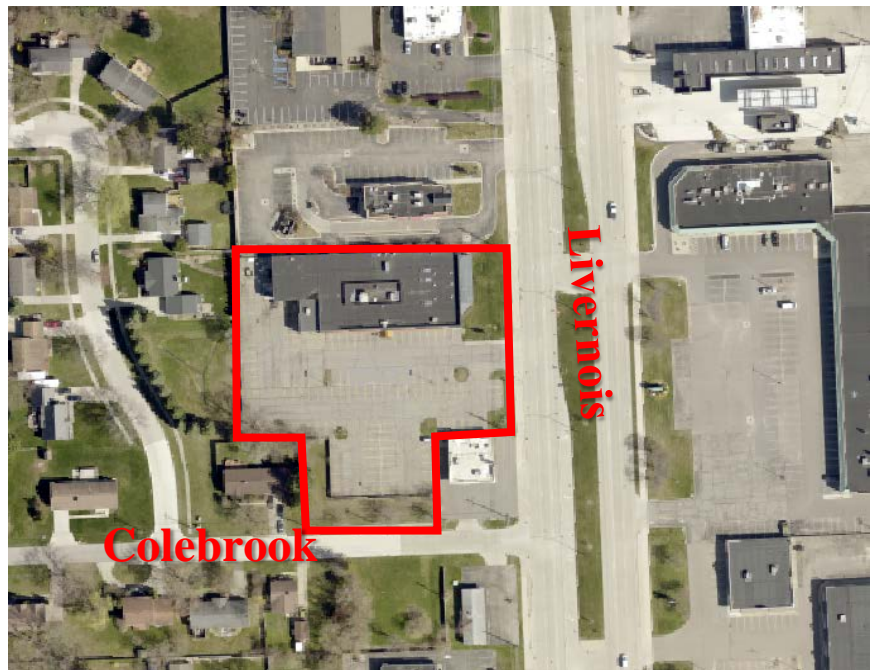
Date: November 8, 2023

Preliminary Site Plan Review For City of Troy, Michigan

Applicant: Dr. Amin
Project Name: Colebrook Road-Curb Cut
Location: West side of Livernois, north of Big Beaver
Plan Date: October 24, 2023
Action Requested: Preliminary Site Plan Approval

BACKGROUND

The owner of the site at located at the northwest corner of Rochester Road and Colebrook Road has requested an additional curb-cut from the site onto Colebrook. From my understanding, a permit to start the work was issued in error, and part of the construction / demolition has started. Once the error was discovered, the City informed the applicant to stop any work.



WORK

These are photos taken from the site of the partially completed work.



PROJECT DESCRIPTION

The applicant is seeking approval to construct a curb cut and drive-aisle to Colebrook through an existing parking lot. The use of the site is for religious services for a place of worship. The applicant notes that during busy times, particularly Friday afternoons, it is difficult to exit the site directly onto Livernois.

The drive-aisle would be one-way exiting only from the site. Access to the new curb-cut and drive-aisle from Colebrook would be blocked with an automatic gate.

The curb-cut aligns with an internal drive in the existing parking lot. The applicant notes that no parking will be lost as a result of the new egress point.

SITE IMPACTS

There is an existing screen wall along Colebrook Road. Portions of the wall will be removed. The applicant has noted that the wall end caps will be replaced once construction is finished.

TRAFFIC

OHM has reviewed the proposed traffic impacts and approves the curb cut.

SITE PLAN REVIEW STANDARDS

Section 8.06 outlines Site Plan Review Design Standards.

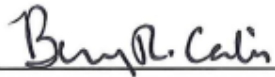
1. Development shall ensure compatibility to existing commercial districts and provide a transition between land uses.
 - a. Building design shall enhance the character of the surrounding area in relation to building and parking placement, landscape and streetscape features, and architectural design.
 - b. Street fronts shall provide a variety of architectural expression that is appropriate in its context and prevents monotony.
 - c. Building design shall achieve a compatible transition between areas with different height, massing, scale, and architectural style.
2. Development shall incorporate the recognized best architectural building design practices.
 - a. Foster a lasting impact on the community through the provision of high quality design, construction, and detailing.
 - b. Provide high quality, durable materials, such as but not limited to stone, brick, glass, and metal. E.I.F.S. or material equivalent shall only be used as an accent material.
 - c. Develop buildings with creativity that includes balanced compositions and forms.

- d. Design roofs that are appropriate to the architectural style of the building and create an appropriate visual exterior mass of the building given the context of the site.
 - e. For commercial buildings, incorporate clearly defined, highly visible customer entrances using features such as canopies, porticos, arcades, arches, wing walls, ground plane elements, and/or landscape planters.
 - f. Include community amenities that add value to the development such as patio/seating areas, water features, art work or sculpture, clock towers, pedestrian plazas with park benches or other features located in areas accessible to the public.
3. Enhance the character, environment and safety for pedestrians and motorists.
- a. Provide elements that define the street and the pedestrian realm.
 - b. Create a connection between the public right of way and ground floor activities.
 - c. Create a safe environment by employing design features to reduce vehicular and pedestrian conflict, while not sacrificing design excellence.
 - d. Enhance the pedestrian realm by framing the sidewalk area with trees, awnings, and other features.
 - e. Improve safety for pedestrians through site design measures.

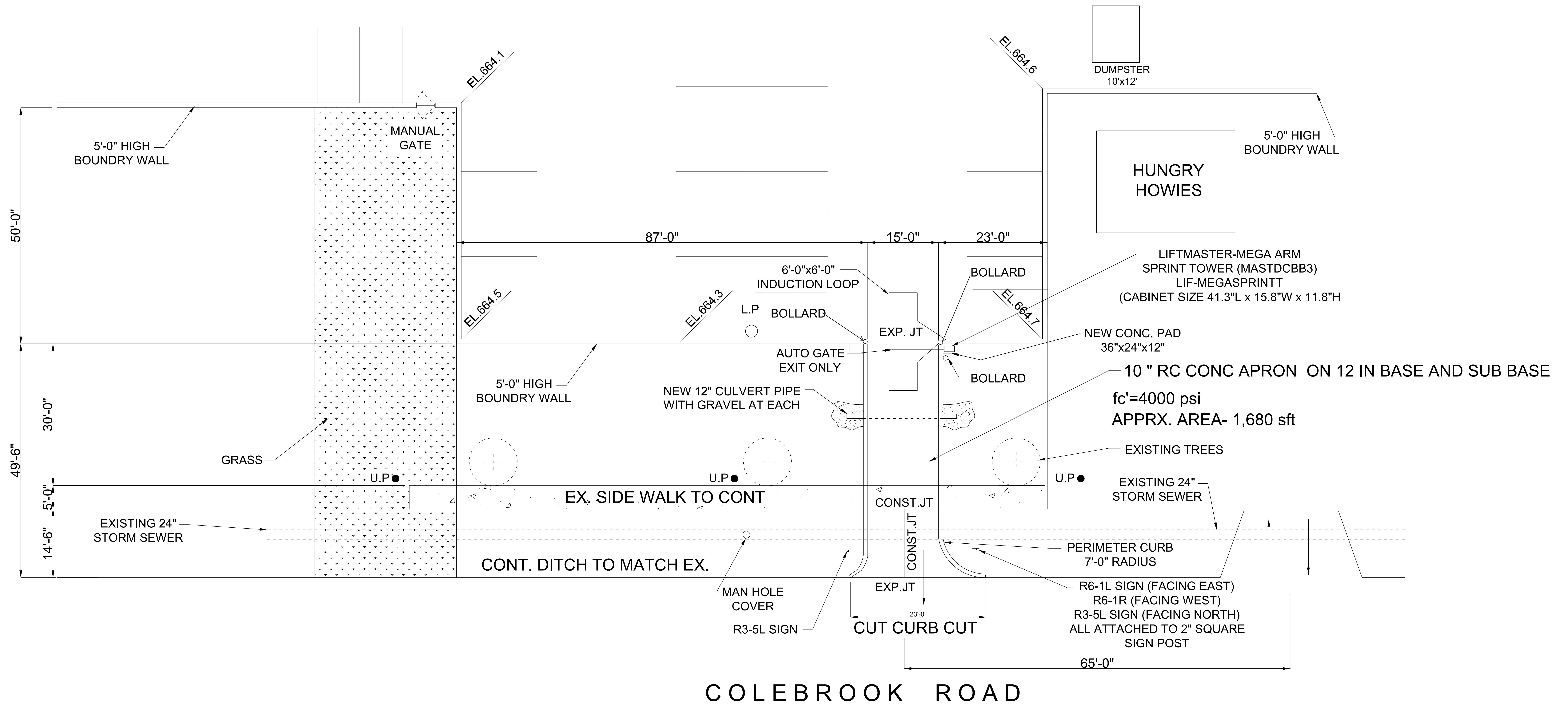
SUMMARY

The Planning Commission should consider if the required site plan standards have been met.

Sincerely,

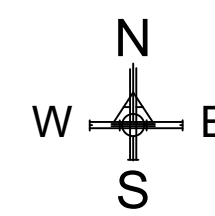


CARLISLE/WORTMAN ASSOC., INC.
Benjamin R. Carlisle, LEED AP, AICP



COLEBROOK ROAD

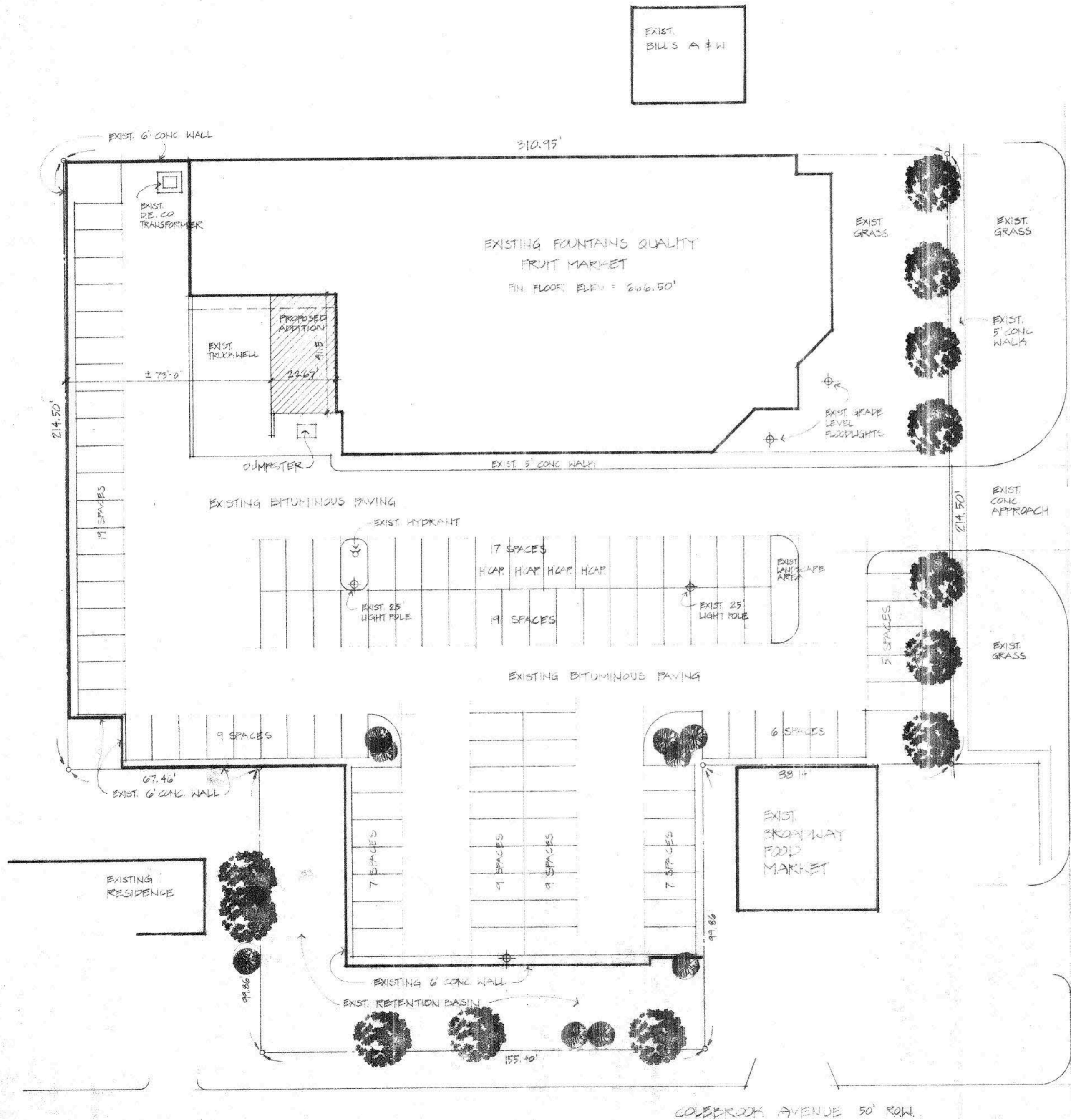
Note: Signs must be a minimum of 6' back of curb.



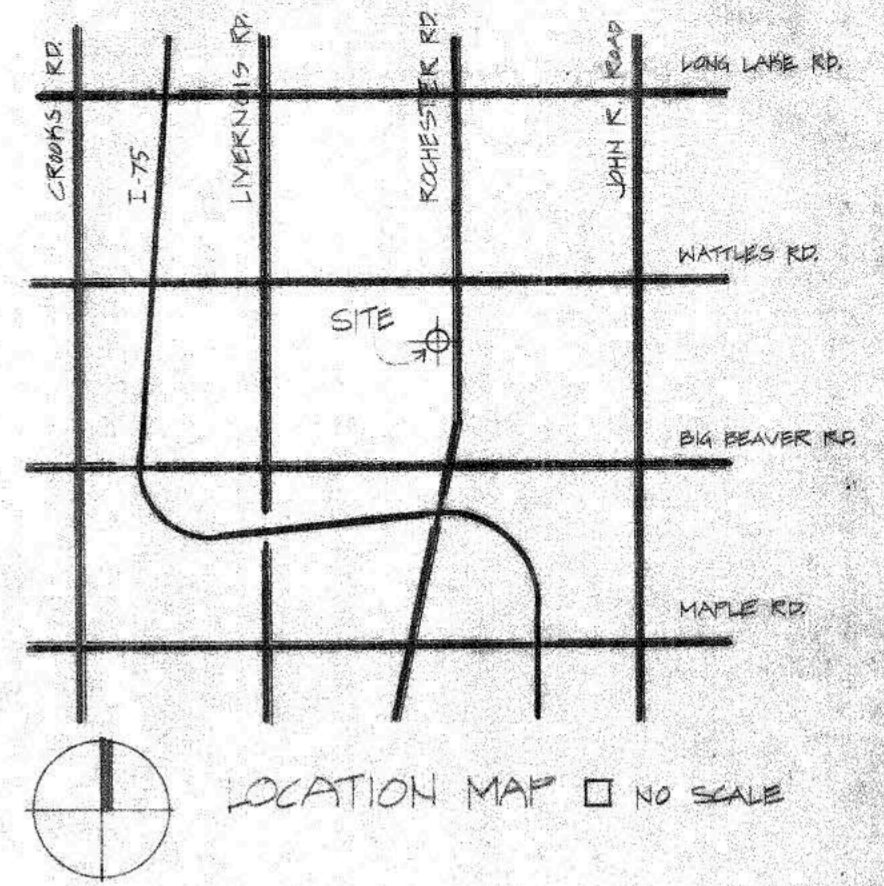
SITE PLAN-ENLARGED VIEW
SCALE 1:200

Notes	
E.P.	Electric post
W.H.	Fire Water hydrant
L.P.	Lamp post
U.P.	Utility pole
	Existing trees

DRAW BY	ENGINEER
Md. D. Hussain	Dr. N. Amin PE
SITE PLAN	NIMA GROUP L.L.C 4700 Rochester Road, Troy, MI 48083 Phone # 248-250-6787
DATE: 9-27-2023	DRAWING NO. NIMA - P 002
	REV. 2



EXIST. BILLS A #1



FIRE PREVENTION DIV. APPROV'D AS NOTED 1/10/86 BY W. NELSON
 1) SUBJECT TO FIELD INSPECTION
 2) SUBMIT SPRINKLER SHOP DRAWINGS
 3) LOCKS REQUIRING KEYS TO EXIT ARE PROHIBITED

CITY OF TROY Building Department		Approved By
Date		
Planning		
Structural		
City		
Electrical	OBTAIN REG'D PERMITS INSTALL TO CODE CALL FOR INSPECTIONS	
Fire		
Building		

APPROVAL OF THIS PERMIT DOES NOT RELIEVE THE THE BUILDER OR OWNER OF COMPLIANCE WITH ALL APPLICABLE CODES AND ORDINANCES. * CERTIFICATE OF OCCUPANCY IS REQUIRED BEFORE THIS BUILDING IS OCCUPIED.

Approved as Noted *Subject to field insp. after completion of addition.*
 CITY OF TROY WATER DEPARTMENT: *WIF + INSP.*
 Date 1-9-86 Approved By: *Wall O.*

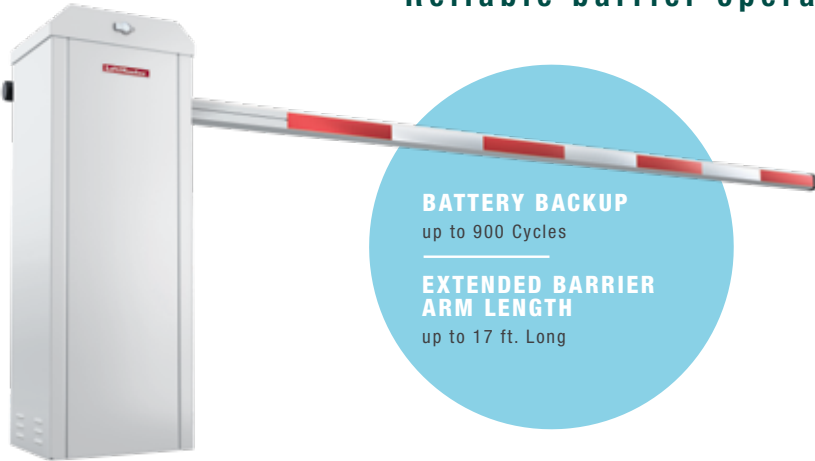


SITE PLAN 1" = 20'-0"

PROPOSED ADDITION TO
 FOUNTAINS QUALITY FRUIT MARKET
 TROY, MICHIGAN
 DEWULF ASSOCIATES ARCHITECTS
 27206 Harper Avenue
 Dearborn, Michigan 48161
 USA
 Telephone (313) 772-6600
 PROJECT NO. 87074

MA/MAT HIGH-TRAFFIC DC BARRIER GATE OPERATOR

Reliable barrier operator for heavy-traffic applications.



MAT

BATTERY BACKUP
up to 900 Cycles

**EXTENDED BARRIER
ARM LENGTH**
up to 17 ft. Long

RELIABLE

HEAVY-DUTY DC MOTOR PERFORMS IN THE MOST DEMANDING APPLICATIONS.

BATTERY BACKUP SAFEGUARDS YOUR PROPERTY, PROVIDING 900 CYCLES WHEN THE POWER IS DOWN.

SMOOTH START/STOP OPERATION EXTENDS LIFE OF OPERATOR.

MAGNETIC LIMIT SENSORS ARE DESIGNED FOR HIGH-CYCLE APPLICATIONS.

SURGE SUPPRESSION PROVIDES INDUSTRIAL SURGE AND LIGHTNING PROTECTION.

WARRANTY OF 10 YEARS FOR ALUMINUM FRAME AND 2 YEARS FOR PARTS.

SMART

SAMS (SEQUENCED ACCESS MANAGEMENT SYSTEM) PROVIDES CONTROL BETWEEN BARRIER AND SLIDE/SWING GATE OPERATOR TO MAXIMIZE TRAFFIC FLOW.

AUXILIARY PROGRAMMABLE RELAY MAKES ADDING ADDITIONAL ACCESSORIES EASY.

MULTI-VEHICLE MEMORY ALLOWS BARRIER ARM TO REMAIN OPEN UNTIL ALL PREAUTHORIZED VEHICLES PASS THROUGH.

SAFE AND SECURE


FIRE-DEPARTMENT COMPLIANCE ALLOWS GATE TO AUTO-OPEN UPON LOSS OF AC POWER OR BATTERY DEPLETION.

BREAKAWAY ARM CAN BE EASILY REINSTALLED IF ARM IS HIT.

ANTI-TAILGATE QUICKLY SECURES PROPERTY, PREVENTING UNAUTHORIZED ACCESS.

SECURITY+ 2.0® SAFEGUARDS ACCESS WITH AN ENCRYPTED TRI-BAND SIGNAL TO VIRTUALLY ELIMINATE INTERFERENCE AND OFFER EXTENDED RANGE.

RECOMMENDED ACCESSORIES:



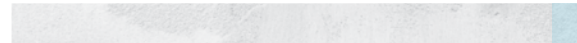
LED BARRIER ARM
MALED12/MALED17
Aluminum arm with integrated red LED light and DOT tape provides exceptional visibility 24 hours a day.



HEAVY-DUTY RETRO-REFLECTIVE PHOTO EYE
AOMRON
Rated up to a 40-foot sensing distance and 24-240VAC/DC, non-monitored.



RED/GREEN TRAFFIC LIGHT
RGL24LY
Assures safe entering and exiting by indicating door status.



LOW-POWER LOOP DETECTOR
LD7LP
Automatic sensitivity settings for ease of vehicle detection. Power-efficient design for max. cycles on battery backup.

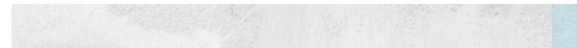


COMMERCIAL ACCESS CONTROL RECEIVER
STAR1000
Standalone commercial receiver with Security+ 2.0 and IP44 housing designed for outdoor environments; can program up to 1,000 remotes for your parking or commercial applications.

OPTIONAL ACCESSORIES:



CONNECTED ACCESS PORTAL, HIGH CAPACITY
CAPXL
Cloud-based access control for residents and guests.



CONNECTED ACCESS PORTAL, 2-DOOR CONTROLLER
CAP2D
Cloud-based, credentialed access control.

MASTERFUL ENGINEERING.

MA/MAT

HIGH-TRAFFIC DC BARRIER
GATE OPERATOR



MA

BASE DIMENSIONS:

MA	41.03" H x 16.37" W x 13.50" D
MAT	41.95" H x 14.46" W x 13.79" D

OPERATOR WEIGHTS (WITHOUT ARM):

Mega Arm Pedestal (MADCBB3)	89 lbs.
Mega Arm Tower (MATDCBB3)	113 lbs.

MECHANICS



- 24VDC/800 RPM Continuous-Duty Motor
- 1/2 HP Equivalent
- Gear Reduction 60:1 Reducer in Synthetic Oil Bath
- Chassis: Powder-Coated 1/4 in. Aluminum Alloy

LED BARRIER ARM SPECIFICATIONS



- Arm Material: Aluminum
- Operating Voltage: 12VDC
- Lamp Type: LED 2528 IP68 Silica Gel Filled
- Color: Red/White
- Power: 14.4 W/m
- UL Listed: UL 325 and UL 991—Class I, II, III and IV
- Warranty: 6-Month Limited

TEMPERATURE SPECIFICATIONS

-13°F (-25°C) to 140°F (60°C)

MALED12	12 ft. x 3 in. x 2-1/4 in.; 17 lbs. (10 ft. 9-3/4 in. Installed)
MALED17	17 ft. x 3 in. x 2-1/4 in.; 25 lbs. (14 ft. Installed)

POWER

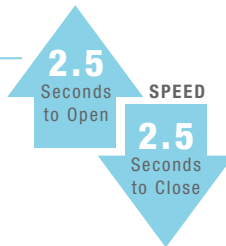


- 120V Applications
- 220V Optional
- Accessory Power 24VDC 500 mA

COMMERCIAL-GRADE DESIGN



- Cover:
 - MA—UV-Resistant Polyethylene
 - MAT—Full Aluminum Cabinet
- UL® 325 and UL 991: Class I, II, III and IV
- Recommended Capacities:
 - Max. Arm Length—9–17 ft. Aluminum or PVC Arm
 - Cycles—6,000/Day



ADDITIONAL ARM OPTIONS.

ALUMINUM

- MA024-10RDOT—Red/White Stripe Articulating Arm (10-ft. Arm Length)
- MALED12—Red/White LED Arm (12-ft. Arm Length)
- 50-ALMARMDOT—Red/White Stripe (15-ft. Arm Length)
- MALED17—Red/White LED Arm* (17-ft. Arm Length)
- MA116RDOT—Red/White Stripe** (17-ft. Arm Length)

PVC

- MA034RDOT—Red/White Stripe Articulating Arm (9-ft. Arm Length)

*Requires Counterweight MALEDCW
**Requires Counterweight MA117

FEATURES.

STANDARD

BUILT-IN 110V CONVENIENCE OUTLET

- Simplifies Adding or Powering Up Accessory Devices and Saves the Expense of Having to Add an Extra Outlet

UNIVERSAL CONTROLLER WITH 8 INPUTS

- Allows for the Connection of a Full Range of Optional External Devices

OPTIONAL

SLIP CLUTCH (MA005C)

- Allows Arm to Be Manually Pushed Open without Damaging the Gearbox or Sheer Pins; Arm Automatically Resets to Normal Closed Position upon Receiving Activation

RIGHT- OR LEFT-HANDED OPERATION

- Provides Flexibility to Fit your Application and Ease of Installation

850LM RADIO RECEIVER

- Universal Receiver for Commercial Applications
- Virtually Eliminates Interference and Improves Range to Access Your Home or Facility

HEATER (MA201/UN201)

- Maintains a Suitable Temperature Inside the Operator When Reaching Below 32°F; 150 W (MA Only) or 500 W (MAT Only)



SUPPORT.



For Support Tools and Training Videos, Visit LiftMasterTraining.com

For More Information on Gate Operators, Visit LiftMaster.com/UL325Gates

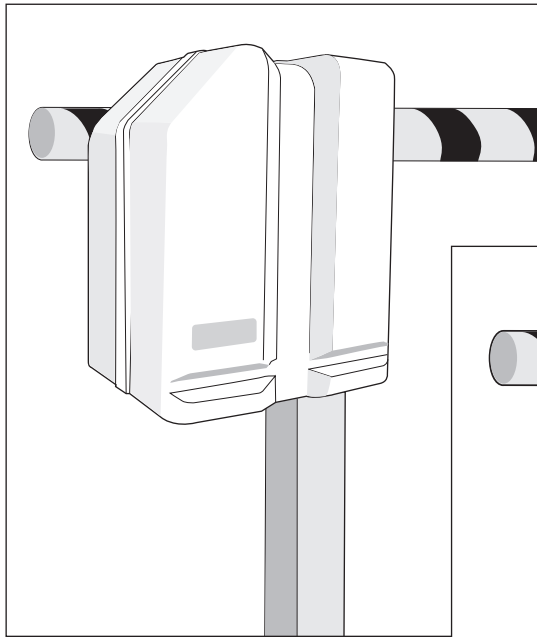
To Find the Right Perimeter Access Solution for You, Visit LiftMaster.com/Solution-Generator



Sales Support: 800.282.6225
Technical Support Center: 800.528.2806
To Order: 800.323.2276

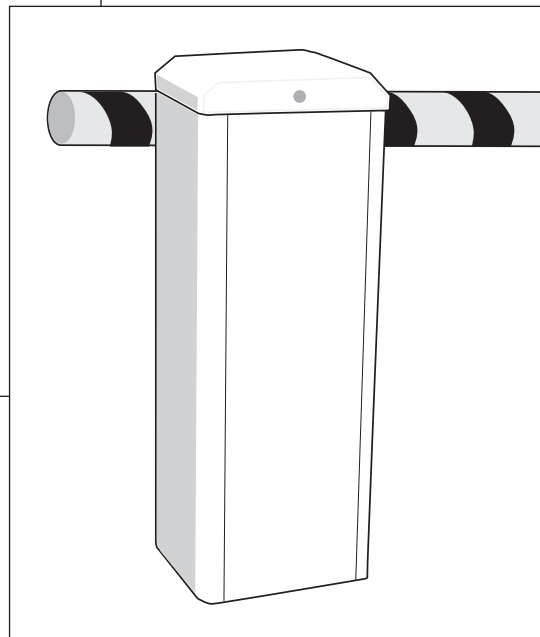
MADCBB, MATDCBB, MASDCBB, & MASTDCBB HEAVY-DUTY DC BARRIER GATE OPERATOR

OWNER'S MANUAL



**MEGA ARM &
MEGA SPRINT**

**MEGA ARM TOWER &
MEGA SPRINT TOWER**



IMPORTANT: Read and understand Warranty Page first. Batteries (included) **MUST** be connected for proper operation of operator. Use (2) LiftMaster 12 Vdc 7AH (Part # MBAT).

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SAFETY » SAFETY SYMBOL AND SIGNAL WORD REVIEW

When you see these Safety Symbols and Signal Words on the following pages, they will alert you to the possibility of **serious injury** or **death** if you do not comply with the warnings that accompany them. The hazard may come from something mechanical or from electric shock. Read the warnings carefully.

When you see this Signal Word on the following pages, it will alert you to the possibility of damage to your gate and/or the gate operator if you do not comply with the cautionary statements that accompany it. Read them carefully.

IMPORTANT NOTE:

- **BEFORE attempting to install, operate or maintain the operator, you must read and fully understand this manual and follow all safety instructions.**
- **DO NOT attempt repair or service of your commercial door and gate operator unless you are an Authorized Service Technician.**

 **WARNING**

MECHANICAL

 **WARNING**

ELECTRICAL

CAUTION

SAFETY » UL325 MODEL CLASSIFICATIONS

UL325 MODEL CLASSIFICATIONS

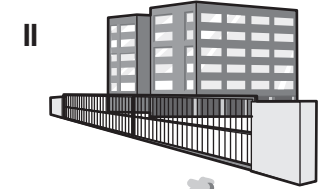
CLASS I – RESIDENTIAL VEHICULAR GATE OPERATOR

A vehicular gate operator (or system) intended for use in a home of one-to four single family dwellings, or a garage or parking area associated therewith.



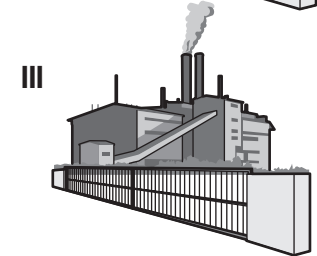
CLASS II – COMMERCIAL/GENERAL ACCESS VEHICULAR GATE OPERATOR

A vehicular gate operator (or system) intended for use in a commercial location or building such as a multi-family housing unit (five or more single family units) hotel, garage, retail store or other building servicing the general public.



CLASS III – INDUSTRIAL/LIMITED ACCESS VEHICULAR GATE OPERATOR

A vehicular gate operator (or system) intended for use in an industrial location or building such as a factory or loading dock area or other location not intended to service the general public.



CLASS IV – RESTRICTED ACCESS VEHICULAR GATE OPERATOR

A vehicular gate operator (or system) intended for use in a guarded industrial location or building such as an airport security area or other restricted access locations not servicing the general public, in which unauthorized access is prevented via supervision by security personnel.



SAFETY ACCESSORY SELECTION

All UL325 compliant LiftMaster gate operators will accept external entrapment protection devices to protect people from motorized gate systems. UL325 requires that the type of entrapment protection correctly matches each gate application. Below are the six types of entrapment protection systems recognized by UL325 for use on this operator.

ENTRAPMENT PROTECTION TYPES

- Type A: Inherent obstruction sensing system, self-contained within the operator. This system must sense and initiate the reverse of the gate within two seconds of contact with a solid object.
- Type B1: Connections provided for a non-contact device, such as a photoelectric eye can be used as a secondary protection.
- Type B2: Connections provided for a contact sensor. A contact device such as a gate edge can be used for secondary protection.
- Type C: Inherent adjustable clutch or pressure relief valve.
- Type D: Connections provided for a control requiring continuous pressure to operate the operator open and close.
- Type E: Built-in audio alarm. Examples include sirens, horns or buzzers.

NOTE: UL requires that all installations must have warning signs placed in plain view on both sides of the gate to warn pedestrians of the dangers of motorized gate systems.



UL325 ENTRAPMENT PROTECTION REQUIREMENTS

UL325 Installation Class	Slide Gate Operator		Swing & Gate Barrier (Arm) Operator	
	Primary Type	Secondary Type	Primary Type	Secondary Type
Class I & II	A	B1, B2 or D	A or C	A, B1, B2, or C
Class III	A, B1 or B2	A, B1, B2, D or E	A, B1, B2 or C	A, B1, B2, C, D or E
Class IV	A, B1, B2 or D	A, B1, B2, D or E	A, B1, B2, C or D	A, B1, B2, C, D or E

The chart above illustrates the entrapment protection requirements for each of the four UL325 classes.

In order to complete a proper installation you must satisfy the entrapment protection chart shown above. That means that the installation must have one primary means of entrapment protection and one independent secondary means of entrapment protection. Both primary and secondary entrapment protection methods must be designed, arranged or configured to protect against entrapments in both the open and close directions of gate travel.

For Example: For a slide gate system that is installed on a single-family residence (UL325 Class I) you must provide the following: As your primary type of entrapment protection you must provide Type A inherent (built into the operator) entrapment sensing and at least one of the following as your secondary entrapment protection: Type B1- Non-contact sensors such as photoelectric eyes, Type B2- Contact sensors such as gate edges or Type D- Constant pressure control.

SAFETY » SAFETY INSTALLATION INFORMATION

1. Vehicular gate systems provide convenience and security. Gate systems are comprised of many component parts. The gate operator is only one component. Each gate system is specifically designed for an individual application.
2. Gate operating system designers, installers and users must take into account the possible hazards associated with each individual application. Improperly designed, installed or maintained systems can create risks for the user as well as the bystander. Gate systems design and installation must reduce public exposure to potential hazards.
3. A gate operator can create high levels of force in its function as a component part of a gate system. Therefore, safety features must be incorporated into every design. Specific safety features include:
 - Gate Edges
 - Guards for Exposed Rollers
 - Photoelectric Sensors
 - Screen Mesh
 - Vertical Posts
 - Instructional and Precautionary Signage
4. Install the gate operator only when:
 - a. The operator is appropriate for the construction and the usage class of the gate.
 - b. All openings of a horizontal slide gate are guarded or screened from the bottom of the gate to a minimum of 4' (1.2 m) above the ground to prevent a 2-1/4" (6 cm) diameter sphere from passing through the openings anywhere in the gate, and in that portion of the adjacent fence that the gate covers in the open position.
 - c. All exposed pinch points are eliminated or guarded, and guarding is supplied for exposed rollers.
5. The operator is intended for installation only on gates used for vehicles. Pedestrians must be supplied with a separate access opening. The pedestrian access opening shall be designed to promote pedestrian usage. Locate the gate such that persons will not come in contact with the vehicular gate during the entire path of travel of the vehicular gate.
6. The gate must be installed in a location so that enough clearance is supplied between the gate and adjacent structures when opening and closing to reduce the risk of entrapment. Swinging gates shall not open into public access areas.
7. The gate must be properly installed and work freely in both directions prior to the installation of the gate operator.
8. Controls intended for user activation must be located at least six feet (6') away from any moving part of the gate and where the user is prevented from reaching over, under, around or through the gate to operate the controls. Outdoor or easily accessible controls shall have a security feature to prevent unauthorized use.
9. The Stop and/or Reset (if provided separately) must be located in the line-of-sight of the gate. Activation of the reset control shall not cause the operator to start.
10. A minimum of two (2) WARNING SIGNS shall be installed, one on each side of the gate where easily visible.
11. For a gate operator utilizing a non-contact sensor:
 - a. Reference owner's manual regarding placement of non-contact sensor for each type of application.
 - b. Care shall be exercised to reduce the risk of nuisance tripping, such as when a vehicle trips the sensor while the gate is still moving.
 - c. One or more non-contact sensors shall be located where the risk of entrapment or obstruction exists, such as the perimeter reachable by a moving gate or barrier.
12. For a gate operator utilizing a contact sensor such as an edge sensor:
 - a. One or more contact sensors shall be located where the risk of entrapment or obstruction exists, such as at the leading edge, trailing edge and post mounted both inside and outside of a vehicular horizontal slide gate.
 - b. One or more contact sensors shall be located at the bottom edge of a vehicular vertical lift gate.
 - c. A hard wired contact sensor shall be located and its wiring arranged so the communication between the sensor and the gate operator is not subject to mechanical damage.
 - d. A wireless contact sensor such as the one that transmits radio frequency (RF) signals to the gate operator for entrapment protection functions shall be located where the transmission of the signals are not obstructed or impeded by building structures, natural landscaping or similar obstruction. A wireless contact sensor shall function under the intended end-use conditions.
 - e. One or more contact sensors shall be located on the inside and outside leading edge of a swing gate. Additionally, if the bottom edge of a swing gate is greater than 6" (152 mm) above the ground at any point in its arc of travel, one or more contact sensors shall be located on the bottom edge.
 - f. One or more contact sensors shall be located at the bottom edge of a vertical barrier (arm).

INTRODUCTION » FEATURES + TOOLS NEEDED + OPERATOR SPECIFICATIONS

FEATURES

- Full service controller with eight inputs and LED indicators for loops, card reader, radio, etc.
- Reversible arm direction for right or left handed operation.
- Instant Reverse Device (IRD) monitor senses obstructions during motion.
- Fail safe (auto open on AC power failure).
- Raise gate input memory will memorize multiple vehicles – ideal for bar code scanners and AVI.
- Ability to STOP arm in close travel if tail-gating is sensed at close loop.
- Anti-tail gate alarm - fires K1 relay to trigger a warning device when tail-gating is sensed.
- SAMS with “memory” - allows Mega Arm to open a slide/swing gate first then raises arm.
- Dynamic motor braking to preserve arm positioning.
- Direct drive gear reducer eliminates many parts that might otherwise fail.
- State of the art MOSFET motor drive technology, NO contactors or relays.
- Soft start and stop in open and close travel motions.
- No limit switches to fail - uses magnetic (Hall Effect) sensors to monitor arm position.
- Maximum Run Timer for motor with anti-tamper protection in closing direction.
- Each operator configurable as primary or second operator.
- LED diagnostics for easy troubleshooting.
- Adjustable Timer-To-Close with on/off selection.
- Transient voltage protection on all inputs.

TOOLS NEEDED

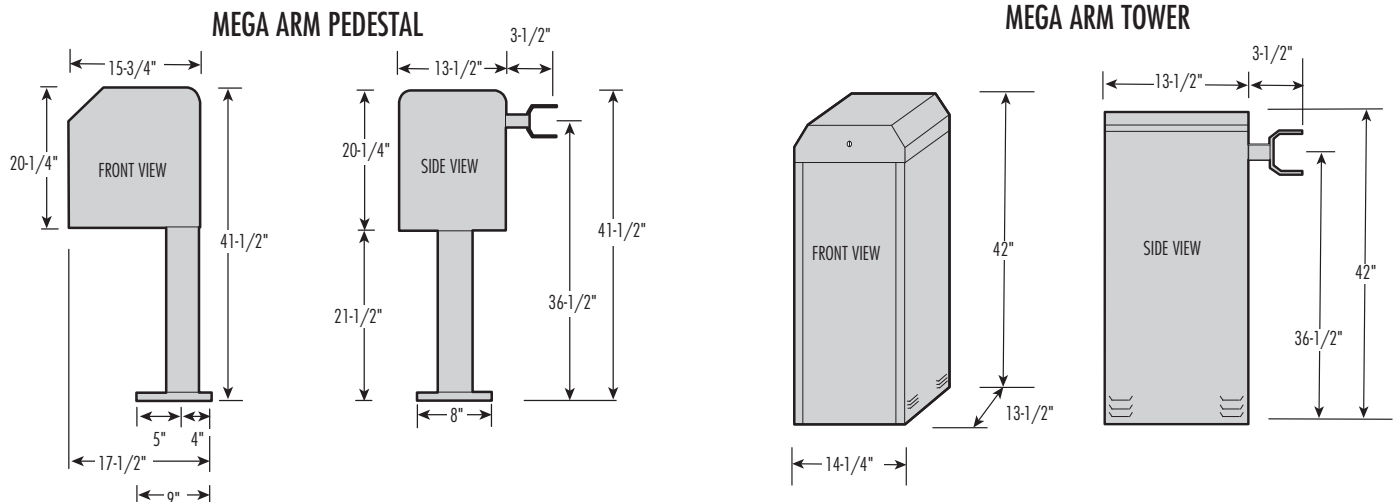
During assembly, installation, and adjustment of the operator, the tools listed below may be needed.

- Wrench or Socket Set
- Phillips Head Screwdriver
- C Clamps
- Level
- Small Screwdriver
- T25 Torx Head Screwdriver

OPERATOR SPECIFICATIONS

This model is intended for use in vehicular gate applications.

- High torque 24 volt Permanent Magnet DC motor
- On 120 Vac installations, unswitched duplex outlet gives convenient supply for 120 Vac accessories
- Built in battery run - inherent 24 Vdc backup power with regulated 24 Vdc for accessories
- Capable of being powered from 120 or 230 Vac, or UL Listed Class 2 Solar Power
- Break away mount design for the 12-15 foot x 3 inch tubular aluminum barrier arm
- Heater option MUST be used if temperature is 30° or below. Heater option available for 120 Vac operators ONLY. Refer to Accessories page.
- All rust proof aluminum construction with white powder coat baked on enamel. Molded Polyethylene UV stabilized cover never needs wax or paint (excludes towers).



INSTALLATION » INSTALL THE CONCRETE PAD AND CONDUIT

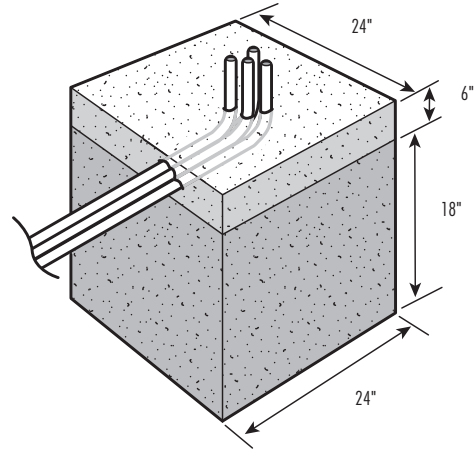
INSTALL THE CONCRETE PAD AND CONDUIT

Check the national and local building codes before installation.

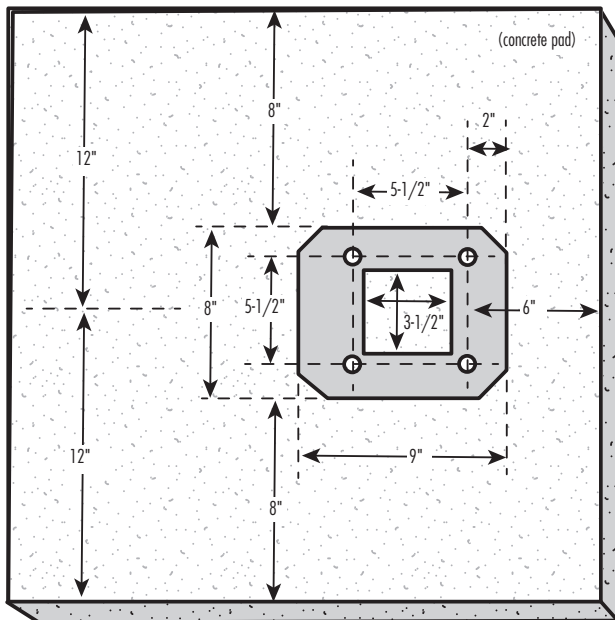
1 Layout the concrete pad for the operator. The concrete pad should be approximately 24" x 24" x 24" in order to ensure proper and stable operation. The pad should be 6" above the finished grade or even with top of curb (if applicable). **NOTE:** Pad should always extend below frost line in regions where ground will freeze.

2 Install the conduit. The conduits should be installed to fit the 3-1/2" x 3-1/2" opening in the pedestal base plate or the 10-1/4" x 8-1/4" opening in the tower base plate. The operator should be centered on the pad and spaced approximately 6" from the edge of the pad on drive way side (in order to get the most reach out of arm). Install conduits for the 120/230 Vac main power, low voltage control wiring, and one or two extra for loop sensor leads. Conduit size should be limited to 1/2" when possible to reduce crowding if more than four are needed. All conduits must be UL approved.

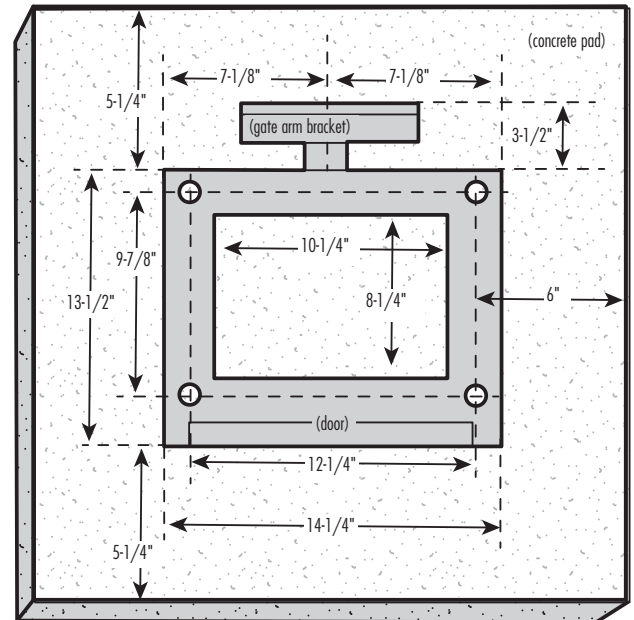
3 Pour a concrete pad (reinforced concrete is recommended).



TOP VIEW OF PEDESTAL FOOTPRINT



TOP VIEW OF TOWER FOOTPRINT

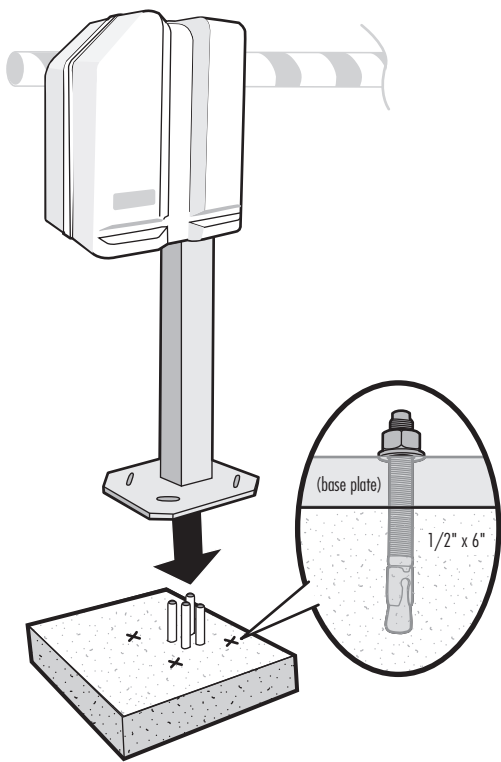


INSTALLATION » ATTACH THE OPERATOR TO THE CONCRETE PAD

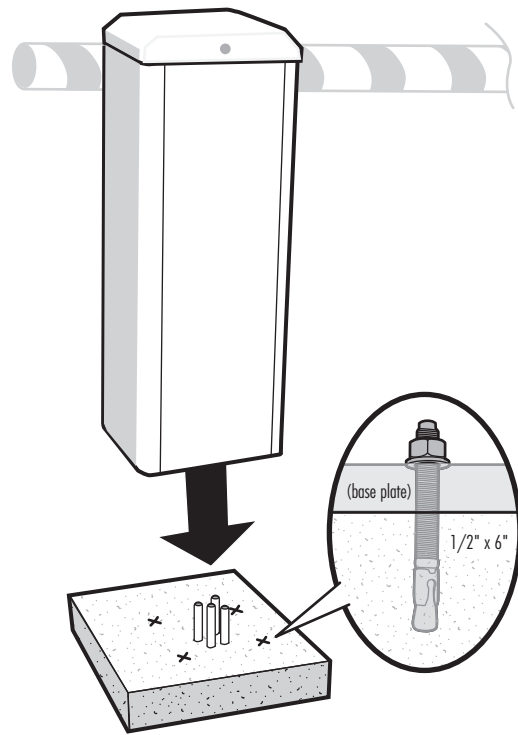
ATTACH THE OPERATOR TO THE CONCRETE PAD

- 1 Position the operator on the concrete pad and mark the mounting holes.
- 2 Install 1/2" x 6" wedge anchors in the location of the mounting holes. The wedge anchors should be installed with approximately 1-1/4" showing above the concrete pad in order to allow for the 1/2" thick base plate as well as washers for leveling.
- 3 Attach the operator to the concrete pad with appropriate fasteners.

PEDESTAL INSTALLATION

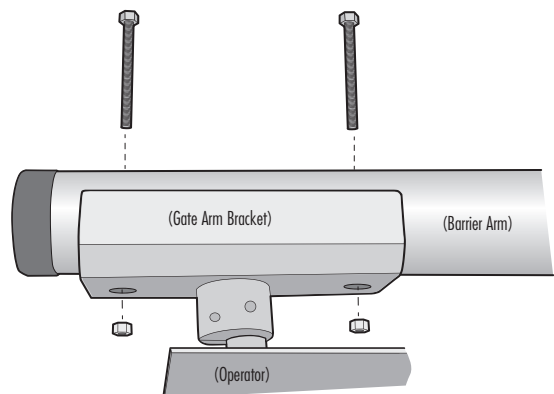


TOWER INSTALLATION



INSTALL THE BARRIER ARM

- 1 Line up the holes in the barrier arm with the slotted holes in gate arm bracket.
- 2 Insert the bolts through the barrier arm and gate arm bracket.
- 3 Secure with the flat washers and nylon nuts (It is recommended to use ONLY nylon nuts).



WIRING » POWER WIRING

WARNING

To reduce the risk of SEVERE INJURY or DEATH:

- ANY maintenance to the operator or in the area near the operator MUST NOT be performed until disconnecting the electrical power and locking-out the power via the operator power switch. Upon completion of maintenance the area MUST be cleared and secured, at that time the operator may be returned to service.
- Disconnect power at the fuse box BEFORE proceeding. Operator MUST be properly grounded and connected in accordance with local electrical codes.
NOTE: The operator should be on a separate fused line of adequate capacity.
- ALL electrical connections MUST be made by a qualified individual.
- DO NOT install any wiring or attempt to run the operator without consulting the wiring diagram. We recommend that you install an optional reversing edge BEFORE proceeding with the control station installation.
- ALL power wiring should be on a dedicated circuit and well protected. The location of the power disconnect should be visible and clearly labeled.
- ALL power and control wiring MUST be run in separate conduit.
- BEFORE installing power wiring or control stations be sure to follow ALL specifications and warnings described below. Failure to do so may result in SEVERE INJURY to persons and/or damage to operator.

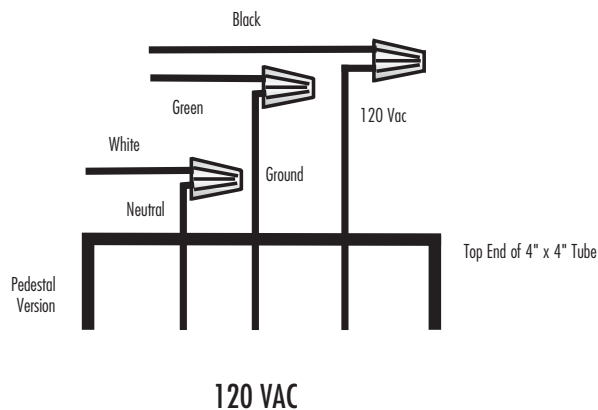
POWER WIRING

Use UL approved 14AWG (or larger) 600 volt insulated wire only.

NOTE: Do not connect the batteries until instructed.

120 VAC

- 1 Ensure your main power is OFF before connecting the AC power.
- 2 The AC wiring should be connected to the wires exiting the conduit or pedestal post. Connect AC power to the operator:
 - Connect the black wire to the incoming 120 Vac hot lead.
 - Connect the white wire to the incoming neutral lead.
 - Connect the green wire to the ground.
- 3 Do not connect any of the AC power wires directly to the control board.



230 VAC

The 120 to 230 Vac conversion kit will need to be purchased for 230 Vac operation. See Accessories page.

WIRING » INPUTS

INPUTS (J5 TERMINAL STRIP)

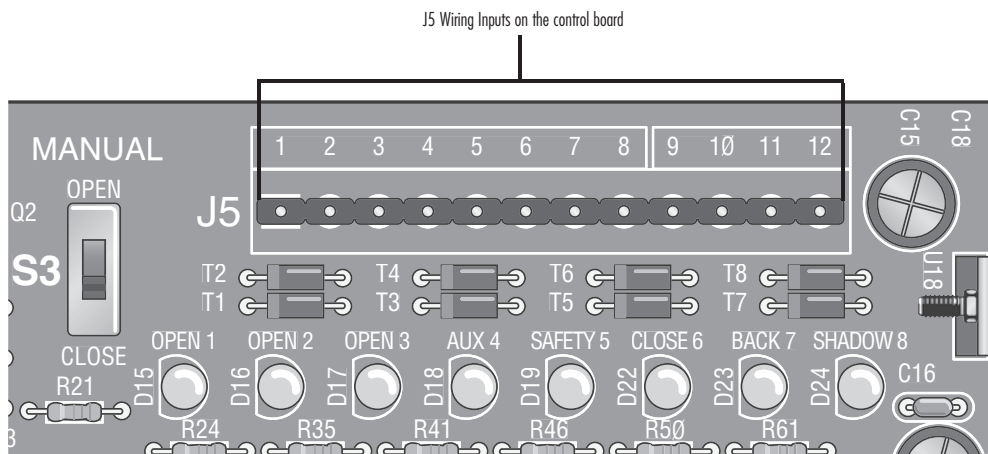
CAUTION

DO NOT connect ANY device which would deliver ANY voltage of ANY kind to these terminals.

The J5 terminal strip is used for controlling the barrier arm with various accessories such as receivers, loop detectors, access controls, and push button stations. Use common and normally open contacts from devices connected to these inputs. The J5 terminal strip is located on the top of the control board. Make connections to the appropriate points for the desired operation. Wires should be UL approved 600 volt rated and at least 18 AWG. They are to be routed through the upper grommet in chassis to avoid chafing. All external control devices must have normally open dry contacts.

TERMINAL	INPUT	EXPLANATION
1,2,3	OPEN	These inputs will trigger gate open when pulsed or hold gate open with maintained contact. When released gate will close if closing timer is on or if close input is given.
4	AUXILIARY OPEN	Same as 1, 2 and 3 with S2 switch 6 off. With S2-6 DIP switch in the ON position, the Multiple Vehicle Memory will activate, with inputs on terminal 4 and Common (COM) on the J5 terminal strip. Use with laser scanners or card readers and (transmitters with timed anti-pass back). With S1 switch 5 on, this input becomes a momentary pulse open, pulse close .
5	SAFETY	This input is generally not used with the MEGA ARM. If used its function is to make gate reverse and go back to the open position if it was closing. Input is disabled when gate is closed.
6	CLOSE	When used with a vehicle detector, it is recommended that the presence contacts (N.O. & C.) be used for the close input. This input will close gate after input is applied and then removed. It will stop the open cycle and reverse gate to close. EXAMPLE: Car crosses over close loop before arm reaches full open position- gate will reverse and close. NOTE: The close input also acts as a safety-stop in that if gate is closing and a tailgater is sensed at the close input, the gate WILL STOP its closing motion and not continue to close until the close input is removed or gate is re-opened.
7	BACK-AWAY (FREE EXIT)	This input is used as a free exit input to open gate. When input is active, gate will open and close immediately once input is removed. EXAMPLE: Car pulls up to exit loop, gate opens; car "backs-away" from exit loop and gate closes.
8	SHADOW (SAMS)	Used to monitor an auxiliary open limit switch of another operator in the same lane. SAMS with memory feature, see page 19.
9,10,11,12	COMMON	These are the commons (0 Vdc) to be used to activate above inputs.

NOTE: Above inputs are tied to LED indicators to show input command activity.

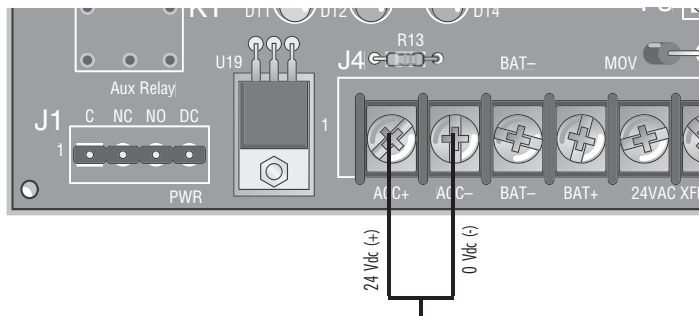


WIRING » ACCESSORY AND RELAY CONNECTIONS + BATTERY INSTALLATION

ACCESSORY AND RELAY CONNECTIONS

These terminals will provide battery backed power to 24 Vdc devices and are located at the bottom of the control board at J4 terminals 1 and 2. Terminal 1 is 24 Vdc (+) and number 2 is 0 Vdc (-). Peripheral CLASS 2 low voltage devices that require 24 Vdc power maybe connected here (500 ma. maximum).

EXAMPLE: Vehicle detector, radio receiver.

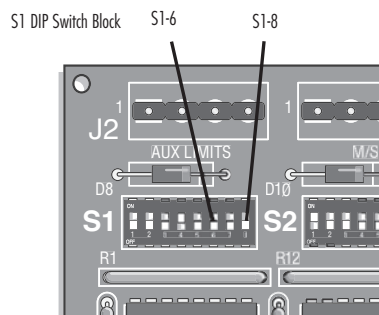


Relay Connection Terminals on the J4 Terminal Block

The K1 Relay (optional) and terminal strip (J1) are used for auxiliary devices such as Counters, Alarms, Buzzers, and SAMS (Sequence Access Management System).

RELAY OUTPUT K1 - (OPTIONAL)

S1-6	S1-8	RESULT
OFF	OFF	Relay will fire (latch) when gate is not closed.
ON	OFF	Relay will fire when arm is pushed up off of limit switch (use with slip clutch option) and fires relay when a tail-gate is detected by the close loop - ANTI TAIL-GATE ALARM.
OFF	ON	Relay will pulse relay when arm reaches full open position.
ON	ON	Relay will only pulse when input is given to J5 1,2,3 inputs. (Refer to pages 14-15.)



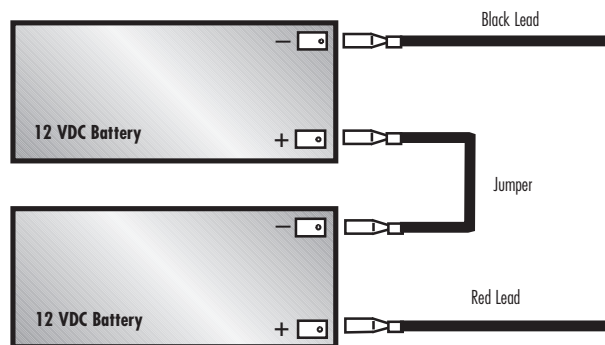
BATTERY INSTALLATION

CONNECTING THE BATTERY LEADS - ALWAYS CONNECT AC POWER BEFORE INSTALLING BATTERIES.

- 1 Connect AC power BEFORE installing batteries.
- 2 Install two new, fully charged 12 volt DC batteries on shelf next to motor.
- 3 Connect red lead from the operator control board to the positive (RED +) terminal of one battery.
- 4 Connect the black lead from the operator control board to the (BLACK -) terminal of the OTHER battery.
- 5 Connect the jumper (provided) between the remaining terminals of each battery if one is not already in place.

IMPORTANT: Do not run operator without installing the batteries.

Replace batteries in pairs using LiftMaster MBAT or 29-NP712 batteries. Failure to install batteries correctly will cause damage and will not be covered by warranty.



WIRING » PRIMARY/SECOND WIRING

PRIMARY/SECOND WIRING

In a primary/second configuration, either operator can be the primary.

- 1 Choose an operator to be the primary and then direct all control wiring to it (also install vehicle detectors and receivers in it).
- 2 At the primary, any input (at J5) with control wires (detectors, receivers, keypads, timers, etc.) to it must also be run to the same terminals of the second. Along with these control wires, both operators **MUST** share a common ground connection from chassis to chassis (or from common to common, i.e., primary gate J5 terminal #12 to second gate J5 terminal #12).
EXAMPLE: If only open and close are used at primary then three wires will run between gates (Figure 1).
- 3 If it is required that if one gate senses an obstruction, the other reverses also, then 3 additional wires must be run between the primary J3 and second J3 (Figure 2). These connections are for transmitting IRD (obstruction signals) between both operators. This will allow the primary or second to inform the other that a closing obstruction has occurred and for it to inform the other that a closing obstruction has occurred and for it to reverse and open. **SET** switches on **S2**, 1-8 the same on both gates.

FIGURE 1

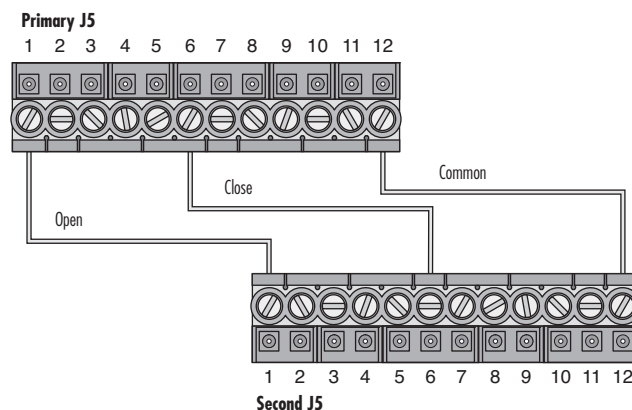
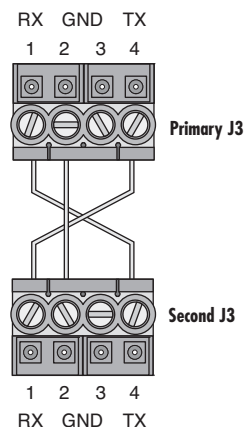


FIGURE 2



IRD - Obstruction Signal Connections
 Terminal 1 of Primary must go to terminal 4 of Second and terminal 1 of Second must go to terminal 4 of Primary. Terminal 2 of Primary must go to terminal 2 of Second.

INSTALL THE RECEIVER » INSTALL THE RECEIVER

⚠ WARNING

To prevent possible **SERIOUS INJURY** or **DEATH** from a moving gate or garage door:

- ALWAYS keep remote controls out of reach of children. NEVER permit children to operate, or play with remote control transmitters.
- Activate gate or door ONLY when it can be seen clearly, is properly adjusted, and there are no obstructions to door travel.
- ALWAYS keep gate or garage door in sight until completely closed. NEVER permit anyone to cross path of moving gate or door.

INSTALL THE RECEIVER

WIRING THE RECEIVER

Contacts 1 and 2 on the receiver terminal strip are for power. The power terminals are unpolarized. Connect terminals 1 and 2 to the accessory power terminals on the J4 terminal strip at the bottom of the control board (Figure 1).

Contacts 3 and 4 on the receiver terminal strip are for a common and a relay. Connect terminals 3 and 4 to terminals 1 and 10 on the J5 terminal strip at the top of the control board.

NOTE: Auxiliary Pin 4 can be used for push to open/push to close functionality.

SET SECURITY MODE

The Universal Receiver can be used with up to 15 rolling code remotes or passwords in HIGH security mode. Alternately, it can be used with up to 31 of any type remote in NORMAL security mode, including any combination of rolling code, billion code (390 MHz only), or dip switch remotes.

The jumper must be set at the HIGH position for the receiver to operate in HIGH security mode. It must be set at NORMAL position to operate at the NORMAL mode (Figure 2).

When changing from NORMAL to HIGH security mode, any previous remote codes must be erased. Repeat Steps 2 and 3 in the Programming Section below to reprogram the receiver for each remote control in use.

The receiver is factory set at HIGH.

PROGRAMMING THE REMOTE TO THE RECEIVER

- 1 Pry open the front panel of receiver case with a coin or a screwdriver. Re-connect power to opener (Figure 3).
- 2 Press and release the "learn" button on the receiver. The learn indicator light will glow steadily for 30 seconds.
- 3 Within 30 seconds, press and hold the button on the hand-held remote that you wish to operate your gate.

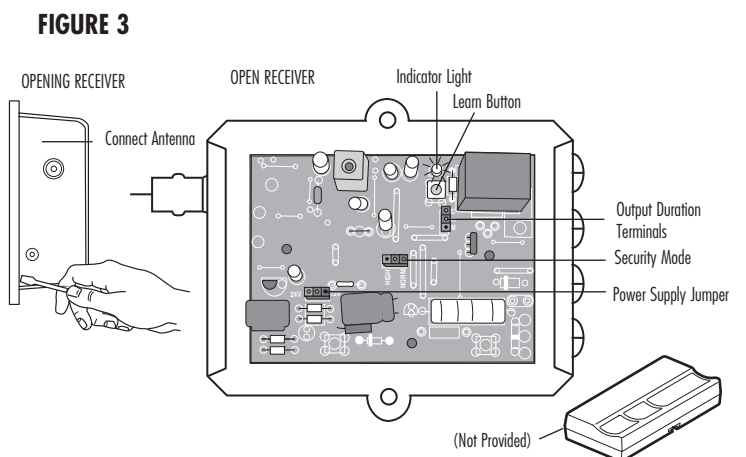
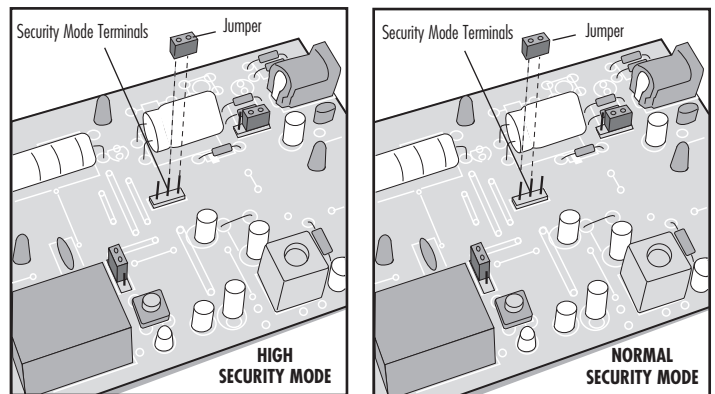
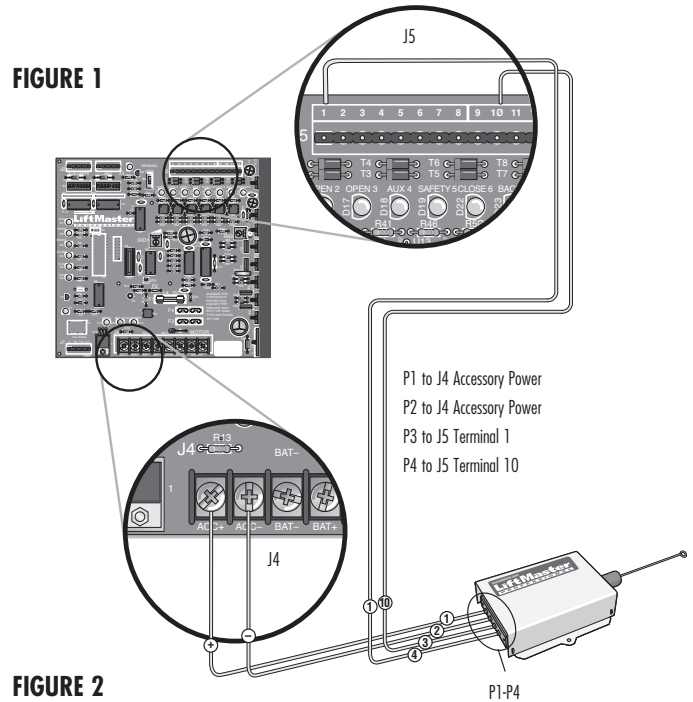
The operator will now operate when the push button on either the receiver or the remote control is pressed.

Repeat Steps 2 and 3 for each remote control that will be used to operate the gate.

TO ERASE ALL REMOTE CONTROL CODES

- 1 Press and hold the "learn" button on the receiver panel until the indicator light turns off (about 6 seconds). All remote codes are now erased.

Follow the steps above to reprogram each remote control.

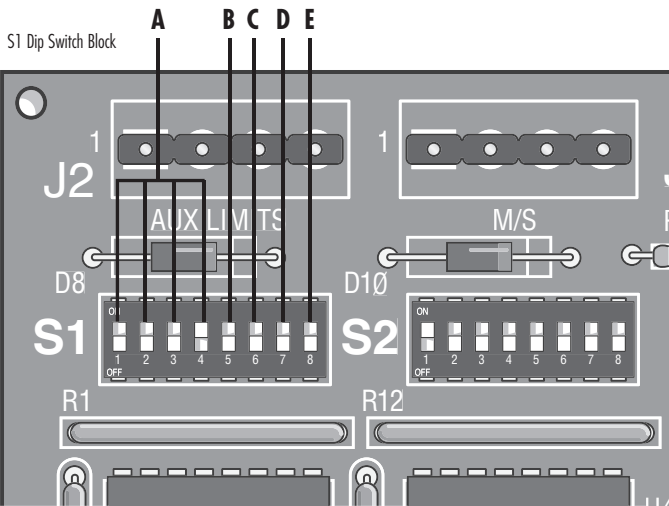


NOTICE: To comply with FCC and/or Industry Canada (IC) rules, adjustment or modifications of this receiver and/or transmitter are prohibited, except for changing the code setting or replacing the battery. THERE ARE NO OTHER USER SERVICEABLE PARTS.

Tested to Comply with FCC Standards FOR HOME OR OFFICE USE. Operation is subject to the following two conditions: (1) this device may not cause harmful interference, and (2) this device must accept any interference received, including interference that may cause undesired operation.

FEATURES AND FUNCTIONS » S1 DIP SWITCHES

S1 DIP SWITCHES



A FAST RUN TIMER (FULL SPEED RUN TIMER) DIP Switch S1-1 to S1-4

When the gate operator activates, it ramps up and slows down for a fixed amount of time, but will run at full speed for variable amounts of time depending upon the settings of the S1-1 to S1-4 DIP switches.

Each DIP switch represents increments of 1/8 second.

When DIP switches S1-1, S1-2 and S1-3 are in the ON position, the Fast Run Timer is set to 2-3/8 seconds by factory default.

When DIP switches S1-1 to S1-4 are set in the OFF position, the full speed run time is 1-1/2 seconds.

The longer the operator runs at full speed, the less ramp up and slow down time. When adjusting, make sure the Fast Run Timer settings DO NOT overrun the slow down time.

B SINGLE BUTTON FUNCTION (INCLUDING PULSE RADIO RECEIVER OPEN/CLOSE) DIP Switch S1-5

With S1-5 DIP switch in the ON position, the Single Button Function (Command to Open/Command to Close) will activate, with inputs on terminal 4 and Common (COM) on the J5 terminal strip. Any of the terminals 9-12 on the J5 terminal strip can be used for common.

When using this feature with the radio receiver (provided), move the radio wire from terminal 1 to terminal 4 on the J5 terminal strip.

C CLUTCH OPTION DIP Switch S1-6

With S1-6 DIP switch in the ON position, and using the Clutch Option; when the barrier arm is manually forced UP (OPEN), the barrier arm will automatically CLOSE.

If the Close Loop detects tailgating, the K1 Relay will activate. If an anti-tailgating alarm is wired into terminal strip (J1), an alarm will sound.

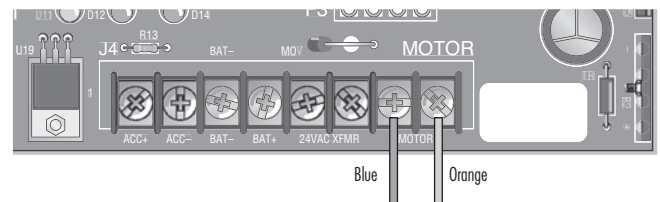
NOTE: When using the Clutch Option, turn DIP switches S1-6 and S2-7 to the ON position (Auto Close). When this feature is activated the barrier arm will CLOSE by the timer whenever it is forced UP (OPEN).

D HANDING THE BARRIER ARM DIP Switch S1-7

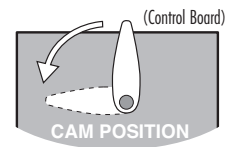
The J4 Motor Wiring is controlled by DIP switch S1-7. The Handing of the Barrier Arm may be changed from right-hand to left-hand operation by reversing the factory default motor connections.

NOTE: Right-hand or left-hand operation is determined by facing the control board with the barrier arm in the CLOSED position. If the barrier arm is to the right, it is set for right-hand gate operation.

1. Disconnect power to the operator.
2. For left-hand operation reverse the motor wires on J4-7 (blue wire) and J4-8 (orange wire) (see below).



3. Set DIP switch S1-7 to the ON position.
4. Turn the motor pulley until the barrier arm is to the left.
5. Turn the Limit Cam so the Limit Cam is parallel to the arm and just behind the limit sensor.
6. Connect power to the operator.



E K1 RELAY (OPTIONAL) DIP Switch S1-8

Auxiliary devices such as Counters, Alarms, Buzzers, and SAMS (Sequence Access Management System), can be wired into the K1 Relay and terminal strip (J1).

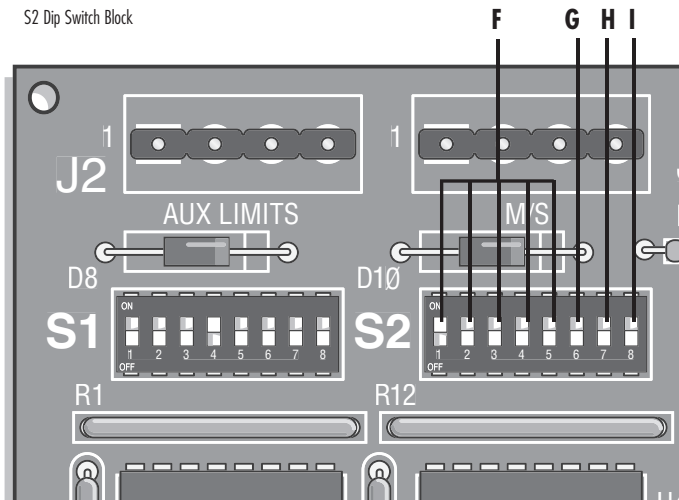
When S1-8 DIP switch is in the OFF position, the K1 Relay will activate throughout the OPEN cycle.

When S1-8 DIP switch is in the ON position, the K1 Relay will be activated briefly until the OPEN LIMIT (OLS) is reached.

FEATURES AND FUNCTIONS » S2 DIP SWITCHES

S2 DIP SWITCHES

S2 Dip Switch Block



F TIMER-TO-CLOSE

DIP Switch S2-1 to S2-5

The S2-1 to S2-5 DIP switches will set the period of time the gate remains opened after reaching the OPEN position.

Each DIP switch represents the number of seconds the gate will remain OPEN before CLOSING. With the S2-3 DIP switch in the ON (factory default) position, there are 4 seconds to allow a vehicle enough time to move out of the path of the closing barrier arm.

Turning multiple switches ON will combine the amount of time the barrier arm remains OPEN.

The maximum hold OPEN time is 32 seconds.

To de-activate this feature turn S2-7 DIP switch to the OFF position.

G MULTIPLE VEHICLE MEMORY

DIP Switch S2-6

With S2-6 DIP switch in the ON position, the Multiple Vehicle Memory will activate, with inputs on terminal 4 and Common (COM) on the J5 terminal strip. Any of the terminals 9-12 on the J5 terminal strip can be used for common.

NOTE: With Multiple Vehicle Memory activated, the barrier arm will remain OPEN until the pre-authorized number of vehicles pass over the Close Loop.

H AUTO CLOSE

DIP Switch S2-7

The S2-7 DIP switch (Auto Close) should be activated in case one or more of the pre-authorized vehicles DO NOT pass through the gate. The barrier arm will close after the set amount of time elapses and the count memory is reset to zero.

I FAIL SAFE (AUTO OPEN ON AC POWER FAILURE)

DIP Switch S2-8

With S2-8 DIP switch in the ON position, the barrier arm will automatically OPEN approximately 15 seconds after a loss of power. Once the power has been restored the operator will resume normal operation after any input.

With S2-8 DIP switch in the OFF position, the barrier arm will resume normal operation until the batteries drop below 50% at which time the barrier arm will OPEN and remain opened until the batteries are fully charged.

ADJUSTMENTS » INSTANT REVERSE DEVICE (IRD) + INSTALL THE BARRIER ARM

⚠️ WARNING

To reduce the risk of **SERIOUS INJURY** or **DEATH**:

- Disconnect power **BEFORE** performing **ANY** adjustments near drive shaft.

INSTANT REVERSE DEVICE (IRD)

Adjustments to be done by qualified service persons only.

The instant reverse device is an internal circuit that continuously monitors the motor's current for increased draw in order to detect obstructions. If the arm is obstructed while closing the arm will reverse to the open position, time out (using the time delay set at S-2 switches 1-5) and then close. If arm is obstructed while opening, the arm will stop, time out (using the time delay set at S-2 switches 1-5) and then close.

Turning the IRD1 right (clockwise more sensitive), or left (counter clockwise less sensitive) in small increments will allow sensitivity adjustments (IF ARM DOES NOT REVERSE, DO NOT CONTINUE TO FORCE).

- 1 Place an obstruction in the path of the arm. Adjust sensitivity so that consistent reversal occurs when the arm hits the obstruction. If the gate stops while opening then the **IRD** is **TOO** sensitive.

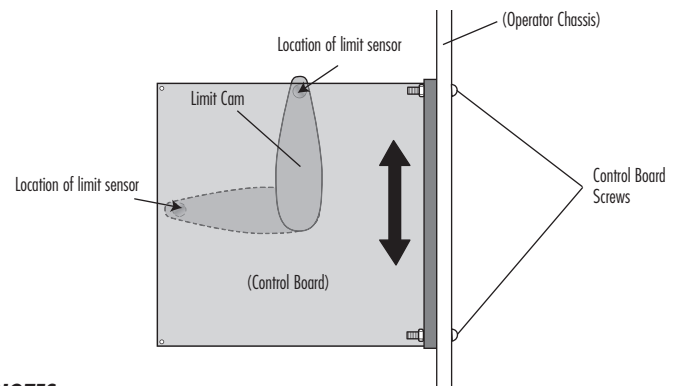
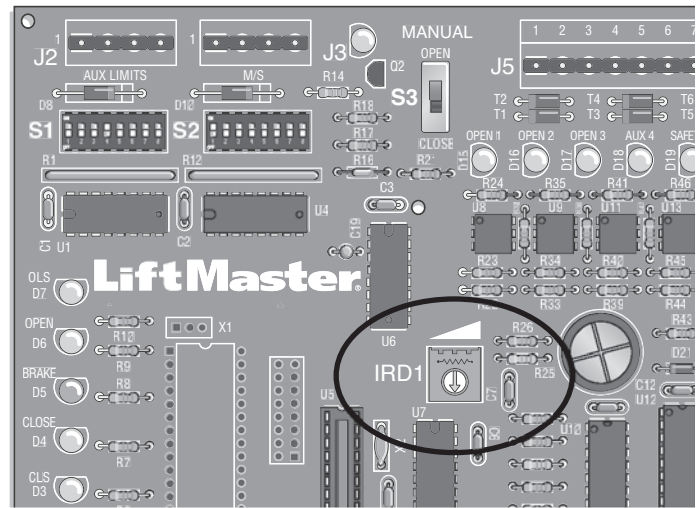
NOTE: Instant reverse device (IRD) should be tested monthly to ensure proper operation.

ADJUST THE BARRIER ARM

The arm should be level in the **HORIZONTAL POSITION**. If the arm is not level it can be adjusted by sliding the control board and/or adjusting the limit cam. The control board has two sensors on the back of the control board that sense when the limit cam has reached the open and close limit.

- **If the arm is closing too far:** Loosen the control board screws and slide the control board **UP** to increase the time it takes for the limit cam to travel between each sensor.
- **If the arm is not closing far enough:** Loosen the control board screws and slide the control board **DOWN** to decrease the time it takes for the limit cam to travel between each sensor.
- **If the arm is not stopping at the limits:** Move the limit cam closer to the control board by loosening the set screw on the limit cam.

After any adjustment open and close the arm to test if the arm is level. Ensure the limit cam set screw and control board screws are tight when adjustments are done.



NOTES:

- In some cases, additional adjustments may be required after the belt wears in.
- To prevent entrapment, allow for two (2) feet minimum clearance past end of arm when in down position.

OPERATION AND MAINTENANCE » IMPORTANT SAFETY INSTRUCTIONS

IMPORTANT SAFETY INSTRUCTIONS

⚠️ ⚠️ WARNING

To reduce the risk of **SEVERE INJURY** or **DEATH**:

1. READ AND FOLLOW ALL INSTRUCTIONS.
2. NEVER let children operate or play with gate controls. Keep the remote control away from children.
3. ALWAYS keep people and objects away from the gate. NO ONE SHOULD CROSS THE PATH OF THE MOVING GATE.
4. Test the gate operator monthly. The gate MUST reverse on contact with a rigid object or stop when an object activates the non-contact sensors. After adjusting the force or the limit of travel, retest the gate operator. Failure to adjust and retest the gate operator properly can increase the risk of INJURY or DEATH.
5. Use the emergency release ONLY when the gate is not moving.
6. KEEP GATES PROPERLY MAINTAINED. Read the owner's manual. Have a qualified service person make repairs to gate hardware.
7. The entrance is for vehicles ONLY. Pedestrians MUST use separate entrance.
8. Disconnect ALL power BEFORE performing ANY maintenance.
9. ALL maintenance MUST be performed by a LiftMaster professional.
10. **SAVE THESE INSTRUCTIONS.**

OPERATION AND MAINTENANCE » GENERAL SERVICE + SHEAR PIN REPLACEMENT + BATTERY

WARNING

To avoid **SERIOUS PERSONAL INJURY** or **DEATH** from electrocution, disconnect **ALL** electric power **BEFORE** performing **ANY** maintenance.

Check at the intervals listed in the following chart:

ITEM	PROCEDURE	EVERY 3 MONTHS	EVERY 6 MONTHS	EVERY 12 MONTHS	EVERY 24 MONTHS
Fasteners	Check and tighten as required		●	◆	
Bearings and Shafts	Check for wear and lubricate	●		◆	
Battery Maintenance	Replace batteries.			◆	●

◆ Repeat **ALL** procedures.

GENERAL SERVICE

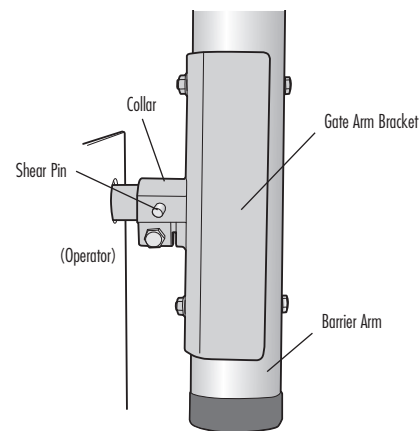
- If the belt is loose or needs replacement, adjust with 4 bolts that support motor to allow 1/4" play.
- Battery voltage should be 27.5 +0.05, -0 Vdc disconnected (set with R63, shown on the Control Board Layout page).

SHEAR PIN REPLACEMENT

If gate arm is vandalized and the tapered pin in the output shaft has been sheared, it must be replaced correctly and with the right pin type. Replacement must be done by always punching out the pin (or pieces) from the small end only. If drilling is required, **DO NOT DAMAGE THE SHAFT**, use a drill bit smaller than the small hole size of the pin. (Correct pin (P/N MA013) is a 2" pin with a number 6 taper only.)

NEVER USE A BOLT AS A TEMPORARY FIX, THIS WILL DAMAGE THE SHAFT AND COLLAR.

1. Turn the S3 Manual Open switch to OPEN on the control board to rotate gate arm bracket to the up position.
2. Disconnect AC power and disconnect batteries.
3. Remove the gate arm bracket and pieces in collar.
4. Drive out pin pieces with hammer and punch (Solid sharp blows are better than light ones).
5. Reinstall gate arm bracket.
6. Lightly oil the new pin and insert into collar.
7. Fully seat pin in shaft by tapping on large end.
8. Reinstall the barrier arm if required.
9. Connect AC power and batteries.
10. Turn off S3 Manual Open switch to CLOSE to put gate into operation.



BATTERY

BATTERY DISPOSAL

Replaced batteries must be treated as a hazardous waste and disposed of in accordance with State, Local and Federal Regulations. See the battery manufacturer's Material Safety Data Sheets (01-30839 "MSDS Sheets, Battery, Standard").

BATTERY REPLACEMENT

Service Kits are available for battery replacement. Please contact Technical Support (see back of this document for contact information). Replace the batteries with LiftMaster P/N MBAT batteries. Replace in pairs.

BATTERY MAINTENANCE / TESTING

The batteries are maintenance free. However, to ensure proper and safe operation, it is recommended that the batteries be replaced every two years. Battery testing is conducted automatically. See the Battery Test Description section for manually initiating the battery test.

BATTERY HANDLING / STORAGE

Refer to the battery manufacturer's Material Safety Data Sheets (01-30839 "MSDS Sheets, Battery, Standard"). LiftMaster does not recommend storage of batteries in the field. Batteries are intended for immediate use.

ADDITIONAL FEATURES » SUGGESTED LOOP SENSOR LOCATIONS

SUGGESTED LOOP SENSOR LOCATIONS

Do not allow control devices to be within 10 feet of gate or operator.

RECOMMENDATIONS:

- If vehicle detectors are used to open or close the gate, use of the presence contacts are recommended. Using the pulse contacts will **REDUCE** the gate's safe operation.
- If closing timer is to be used, use **ONLY** on a dedicated free exit.
- Close loop must be centered under gate arm.

FREE EXIT ON VEHICLE APPROACH

Gate will open when sensed by exit loop and then close once the close loop is cleared. If the vehicle pulls up to the exit loop and then backs away, it will close.

Space between loops will be 4 to 10 feet.

Terminal #7 is backaway (free exit).

Terminal #6 is close input.

ENTRY WITH ACCESS CONTROL DEVICE

Gate will open when activated by an access control device. When vehicle passes and clears close loop, gate will close.

NOTE: If a second vehicle tail-gates and is sensed at the close loop, gate will stop its closing motion until loop is cleared again.

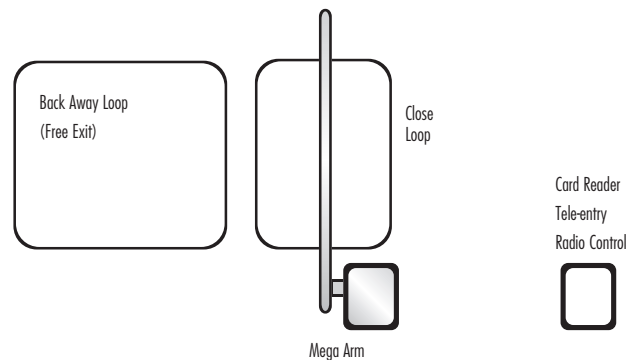
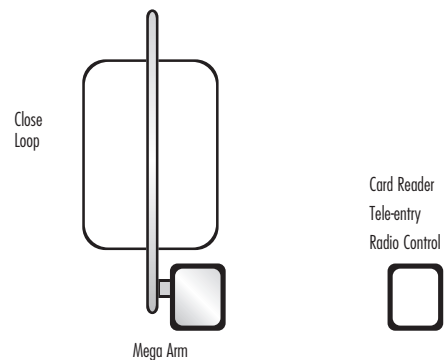
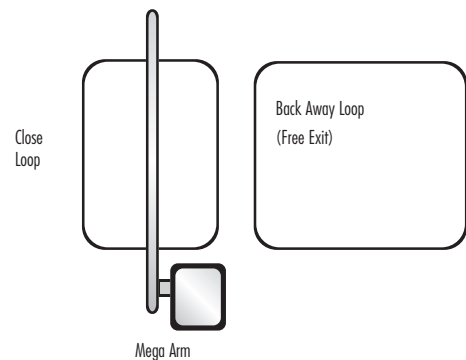
Terminal #6 is close input.

Terminals #1, 2, and 3 are open inputs.

DUAL DIRECTION AS ENTRY OR FREE EXIT

Dual direction is a combination of both of the above configurations to provide the ability for traffic to enter or exit in the same lane.

Space between loops will be 4 to 10 feet.



ADDITIONAL FEATURES » TRAP INSTRUCTIONS

TRAP INSTRUCTIONS

INSTALL THE K1 AUXILIARY RELAY AND CONNECTOR AT MEGA ARM CONNECTED TO THE ACCESS DEVICE

1. Press the relay into the K1 location ensuring the pins are properly aligned.
2. Press the connector into the J1 connector pins.

INSTALL THE K1 AUXILIARY RELAY AND CONNECTOR AT THE SECOND

1. Press the relay into the K1 location ensuring the pins are properly aligned.
2. Press the connector into the J1 connector pins.

WIRE THE CONNECTIONS BETWEEN THE OPERATORS

1. Connect the Normally open output (NO) of the K1 relay on the trap operator to the OPEN input (J5 - term#2) of the second operator.
2. Connect the common output (C) of the K1 relay of the trap operator to the common of the second operator (J5 - term#12).
3. Connect the Normally open output (NO) of the K1 relay on the second operator to the INTERLOCK input of the trap operator (J5 - term#8).
4. Connect the common output (C) (J5 - term#12) of the second operator to the common of the trap operator (J5 - term#12).

SET THE DIP SWITCHES AT THE TRAP OPERATOR

1. Set switch bank S1 to 00100001 where 1 is up and 0 is down.
2. Set switch bank S2 to 00100010 where 1 is up and 0 is down.

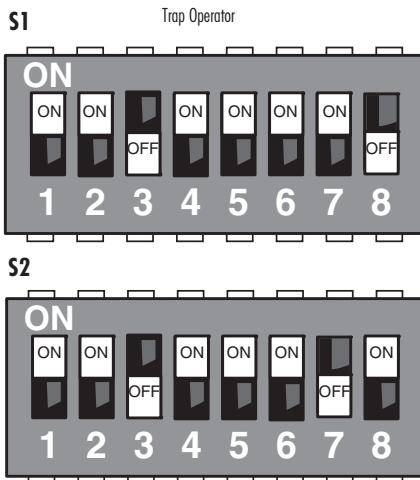
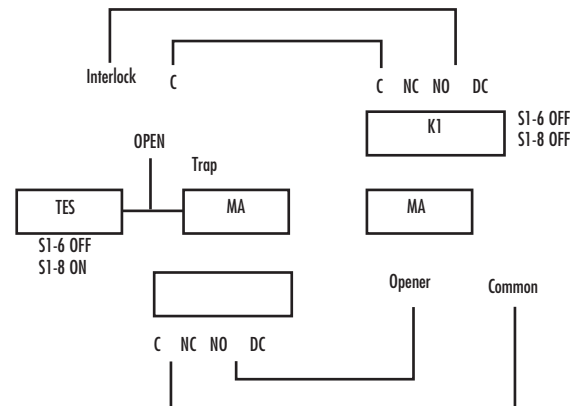
SET THE DIP SWITCHES AT THE SECOND OPERATOR

1. Set switch bank S1 to 00100000 where 1 is up and 0 is down.
2. Set switch bank S2 to 00100010 where 1 is up and 0 is down.

RECONNECT THE POWER AND TEST

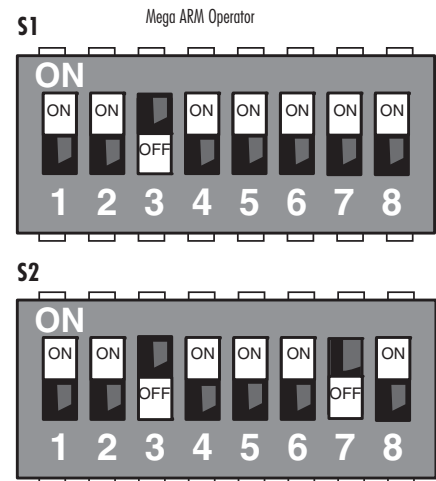
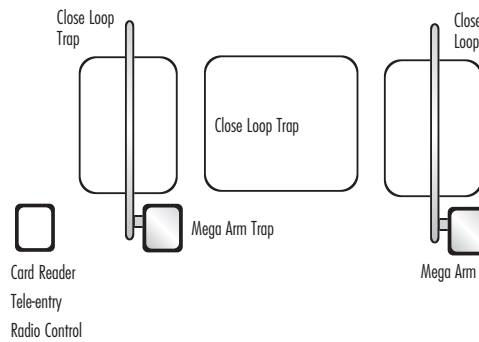
1. Reconnect the DC power by replacing the neutral (Black) wire to the battery terminal.
2. Reconnect the AC Power to the operator.
3. To test, activate the following sequence of inputs:
 - Open the trap gate using the access device.
 - When the trap gate is open, activate the close loop on the trap operator. The trap gate will close and the second gate should open.
 - When the second gate is open, activate the close loop on the second operator. The second gate should close.

TRAP SET UP



TRAP CONFIGURATION

Must use trap kit. See Mega Arm Options Parts List



ADDITIONAL FEATURES » SEQUENCE ACCESS MANAGEMENT SYSTEM (SAMS) WITH "MEMORY"

SEQUENCE ACCESS MANAGEMENT SYSTEM (SAMS) WITH "MEMORY"

SAMS WITH OTHER OPERATORS

REQUIRES THE K1 RELAY OPTION (Order SAMS KIT)

This feature allows a logical interface between the MEGA ARM barrier gate and a swing, slide, etc. gate operator (or MTC-31). All that is required is 4 wires between the MEGA ARM barrier gate and the other operator. It will be necessary to have one set of dedicated/isolated dry contacts - {C. and N.C.} COMMON and NORMALLY CLOSED be available at the other operator's OPEN LIMIT SWITCH. Most operators will require that this EXTRA limit switch be added to their open limit switch assembly.

OPERATION

A one second pulse from access control device to the MEGA ARM will energize its K1 relay sending an open signal to the other operator causing it to open. However, the MEGA ARM's barrier arm will not raise yet. When the other operator reaches its full open limit switch, this will open the COMMON and NORMALLY CLOSED contact on the EXTRA open limit switch. This will allow the original signal from your access control device (that was stored in memory) to now raise the barrier arm. As long as the other operator is in the full open position, any additional open pulse sent will in turn energize the MEGA ARM's K1 relay to send another open signal to the other operator as well as cause the arm to raise again if it has closed via a car crossing the MEGA ARM's close loop.

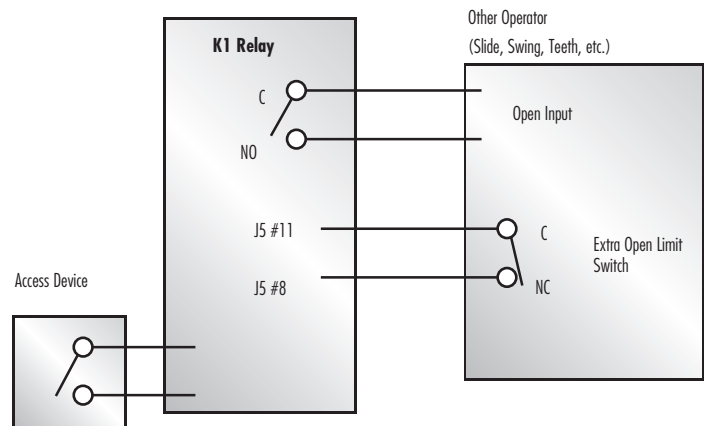
WIRING:

1. Run 2 wires from the other operator's isolated common & normally closed contacts of its open limit switch to the MEGA ARM J5#8 and one of the commons J5, #9-12.
2. Run 2 wires from the MEGA ARM's K1 relay (common & normally open) to the other operators common and open input. (**WARNING:** Max of 30 VOLTS at .5 amps through relay). J5 #8 was the unused SHADOW LOOP input on the MEGA ARM. **NOTE:** A separate open device (24 hour timer, toggle switch) can be run to the other operator to control it without raising the gate arm. Tampering with the other operator's safety loops, safety edges and reverse sensors WILL NOT cause the arm to raise if one tripped. The arm will only raise if an intended open signal was sent to the MEGA ARM.

NOTES:

- For motorized teeth, vertical pivot or overhead operator, leave S1-6, S1-8 OFF (this will keep the K1 relay latched down until the arm reaches the down position. This will keep the other gate operator locked open or teeth locked down until the arm closes completely).
- In this mode, if the arm senses an impact, the K1 relay will stay energized holding open (or teeth down) the other operator until the arm times out and closes.

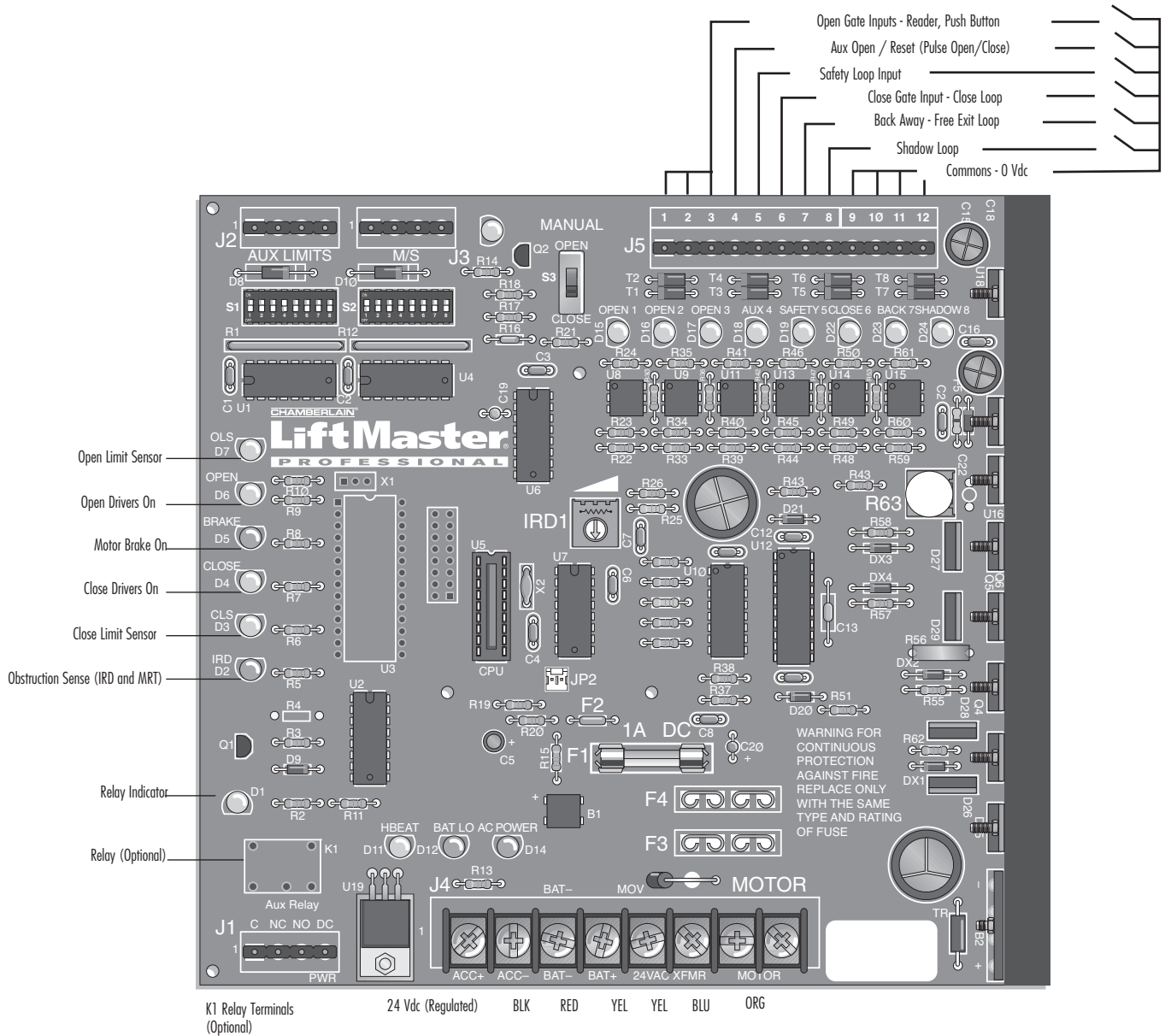
NOTE: Insert a jumper across the JP2 terminal to allow the SAMS feature to work with the multiple vehicle memory count selection, use the K1 relay to open the sequenced gate (S1-5 off, S2-6 on, jumper across JP2). This allows gate to store input counts via J5 #4 but not raise the arm until the sequenced slide or swing gate has fully opened.



S1, #6 and #8 (ON) = K1 Relay PULSE only.
S1, #6 and #8 (OFF) = K1 Relay LATCH only.

ADDITIONAL FEATURES » CONTROL BOARD LAYOUT + INPUT LOCATIONS

CONTROL BOARD LAYOUT



INPUT LOCATIONS

Accessory power is 24 Vdc regulated rated at 500 ma. [1/2 amp].

NOTE: J5 #8 is now the SAMS with memory input (see page 13).

D11: Heart beat. Shows that processor and program are running properly.

D12: Battery status. See diagnostic procedures.

D14: AC power indicator. Shows that AC power is present.

S3: Manual open. To allow gate to be opened or closed during service of operator. Keep in the "Close" position for normal operation.

F3: 10 amp ATO type fuse for 24 Vac input power. (UL listed fuse only.)

F4: 15 amp ATO type fuse for 24 Vdc battery input power. (UL listed fuse only.)

TROUBLESHOOTING » BATTERY CHECKOUT + GATE NOT OPERATING

WARNING - DISCONNECT BATTERIES AND AC POWER BEFORE SERVICING ANY MECHANICAL OR MOVING COMPONENTS.

BATTERY CHECKOUT

When the batteries become weak the gate will begin to run noticeably slower.

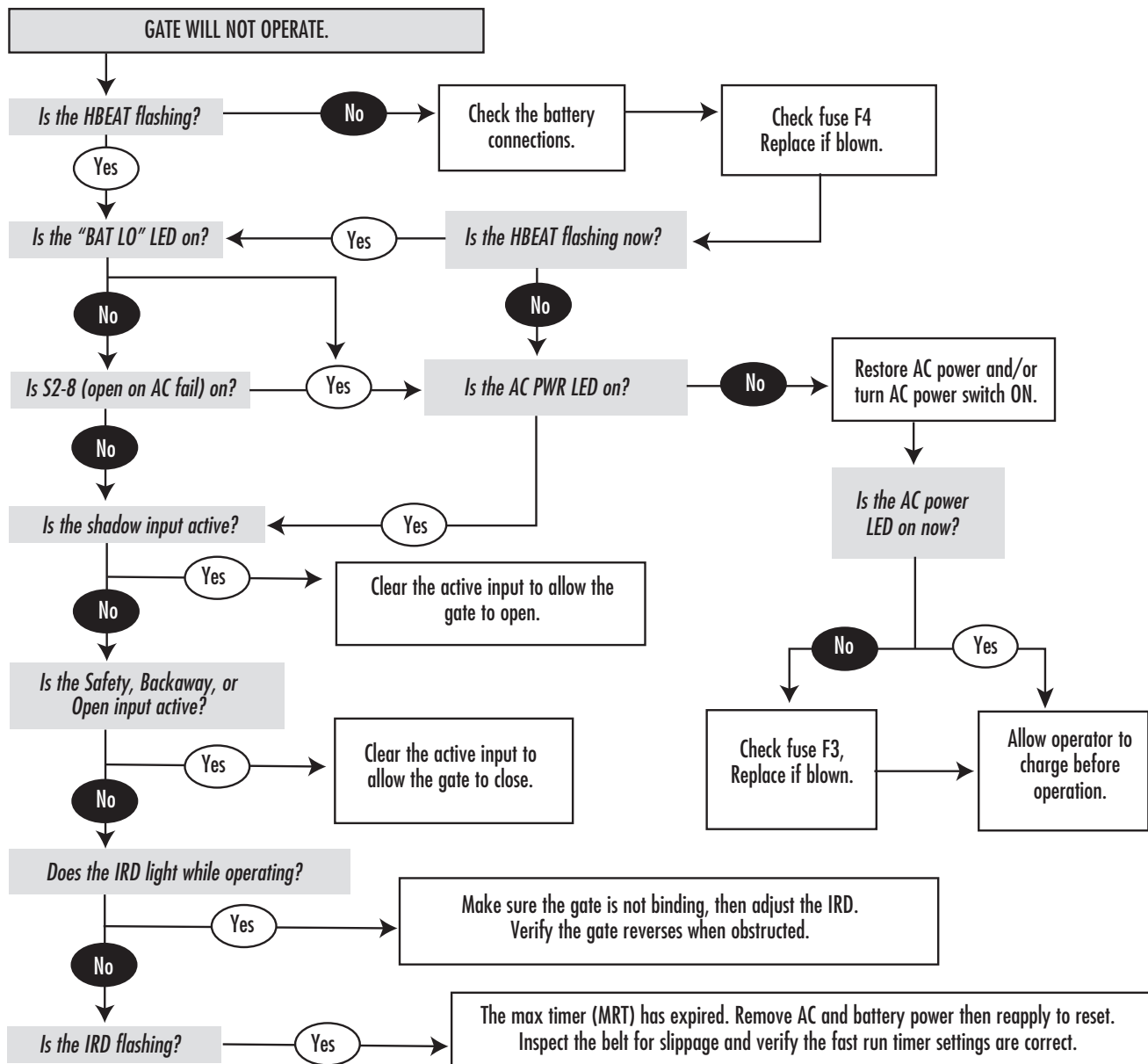
NOTE: Batteries should only be checked when you are sure they have had adequate time to fully charge.

1. Turn off the AC power and run gate for 5 to 10 cycles while observing low battery indicator LED D12. If LED 12 comes ON, batteries are too weak to function properly. If LED 12 does not light, then voltage should be checked as they still maybe near failure. Correct voltage is approximately 24.5 Vdc.

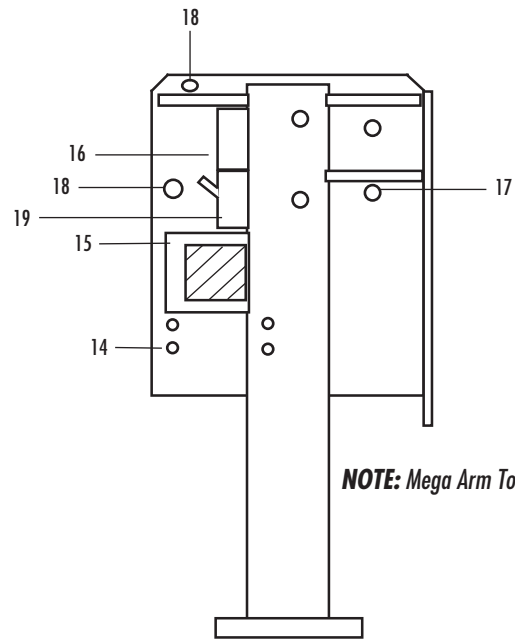
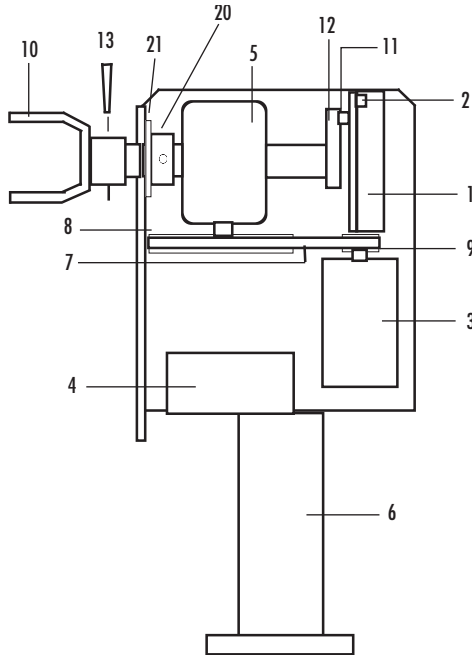
NOTE: If LED D12 does light, gate will open to conserve batteries in this test or in a real power loss, even if mode switch 8 on S2 is off.

Return of AC power will clear low battery indicator. Correct charge voltage is 27.5 Vdc with batteries not connected (set with R63, shown on the Control Board Layout page).

GATE NOT OPERATING



REPAIR PARTS » PART NUMBERS AND DESCRIPTIONS + PARTS SHIPPED + MEGA ARM TOWER UNIQUE PARTS LIST + MEGA ARM OPTIONS PARTS LIST



NOTE: Mega Arm Tower not shown

PART NUMBERS AND DESCRIPTIONS

ITEM	PART NUMBER	DESCRIPTION	ITEM	PART NUMBER	DESCRIPTION
1	MA001	Controller	14	MA014	Bolt and Nut (4) Motor
2	MA002	Removable Connector	15	MA015	Transformer
3	MA003	DC Motor - 24 Vdc	16	MA016	120 Vac Duplex Outlet (120 Vac Only)
4	MBAT	12 Vdc 7AH Battery 2 required	17	MA017	Bolt and Nut (4) Reducer
5	MA005	Gear Reducer 60:1	19	MA019	On/Off Switch
6	MA006	Aluminum Chassis	*	MA020	Operator Cover for Mega Arm (Not Tower)
7	MA007	Drive Belt	*	MA021	Nylon Arm Nuts (2)
8	MA008	Reducer Pulley	*	MA022	Arm Bolts (2)
9	MA009	Motor Pulley	*	MA023	Gate Arm - 12'
10	MA010	Gate Arm Bracket	*	73A3	Filter Module
11	MA011	Magnet	*	74-31243	Surge Suppressor
12	MA012	Cam Arm	20	MA036	Collar
13	MA013	Shear Pin	21	MA037	Nylon Washer

(*) parts not shown

PARTS SHIPPED

ITEM	QTY
MEGA ARM Operator Controller	1
Operator Cover	1
Installation and Service Manual	1
Arm Bolts with Washers	2
Nylon Nuts	10
7AH Batteries	2

MEGA ARM TOWER UNIQUE PARTS LIST

ITEM	PART NUMBER	DESCRIPTION
*	MA020T	Operator Cover for Mega Arm Tower
*	MA020D	Operator Door for Mega Arm Tower

MEGA ARM OPTIONS PARTS LIST

ITEM	PART NUMBER	DESCRIPTION
*	71-TRAP	Trap option
*	71-SPRINT	Sprint option
*	71-TRAPSP	Sprint Trap option

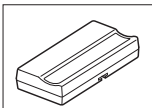
ACCESSORIES

REMOTE CONTROLS

Chamberlain offers a variety of LiftMaster Security+® and Passport™ remote controls to satisfy your application needs. Single-button to 4-button, visor or key chain. Additionally, Passport™ remote controls are ideal for integration with Telephone Entry and Access Control Systems. Contact your authorized LiftMaster dealer for details.

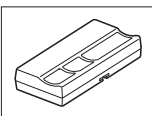
SECURITY+® SINGLE BUTTON REMOTE CONTROL

Includes visor clip. Model 371LM



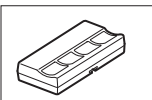
SECURITY+® 3-BUTTON REMOTE CONTROL

Includes visor clip. Model 373LM



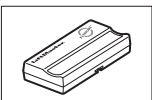
SECURITY+® 4-BUTTON REMOTE CONTROL

Includes visor clip. Model 374LM



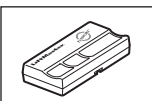
PASSPORT™ 1-BUTTON REMOTE CONTROL

Includes visor clip. Model CPT13.



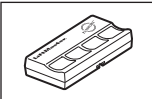
PASSPORT™ 3-BUTTON REMOTE CONTROL

Includes visor clip. Model CPT33



PASSPORT™ 4-BUTTON REMOTE CONTROL

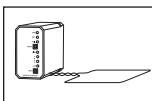
Includes visor clip. Model CPT43



MISCELLANEOUS

24 VDC LOOP DETECTOR

Model A24



WIRING HARNESS

For the A24. Model A57

HEATER KIT

500 watt with thermostat (MAT and MATS only). Model UN201

SLIP CLUTCH OPTION

For Mega Arm Gear Box. Model MA005C

K1 RELAY OUTPUT OPTION

Model MA200

ALUMINUM ARM

White, 12' x 3" diameter with warning labels. Model MA023

ALUMINUM ARM:

12' x 3" diameter with yellow/black stripes. Model MA024

ALUMINUM ARM

12' x 3" diameter with reflective yellow/black stripes. Model MA024R (Highly Recommended)

SAMSKIT

Includes required relay and limits.

NYLON ARM NUTS

(Pkg. of 50). Model MA021

NYLON ARM NUTS

(Pkg. of 50), thin. Model MA021A

ADAPTER COLLARS

For padded arm option (2 included). Model MA031

ROUND PADDED ARM

12' x 4" diameter, yellow (requires MA031). Model MA025

REPLACEMENT PAD:

12' x 4" diameter, yellow. Model MA026

REPLACEMENT ARM TUBE

12' x 2" diameter. Model MA027

ROUND PADDED ARM

14' x 4" diameter, yellow (requires MA031). Model MA028

REPLACEMENT PAD

14' x 4", yellow. Model MA029

REPLACEMENT ARM TUBE

14' x 4". Model MA030

COUNTER WEIGHT

Required for all 15' arms. Model MA117

ALUMINUM GATE ARM

White, 15' x 3" (requires MA117). Model MA115

ALUMINUM GATE ARM

15' x 3" with yellow/black stripes (requires MA117). Model MA116

ALUMINUM GATE ARM

15' x 3" with reflective yellow/black stripes (requires MA117). Model MA116R

ARTICULATING PVC (FOLDING) ARM

9' with hardware kit. Model MA034 (Highly Recommended)

HARDWARE KIT

(Only for MA034). Model MA033

PVC ARM: 9'

(Only for MA034). Model MA035

ARTICULATION ALUMINUM (FOLDING) ARM:

10' without hardware kit, with yellow/black stripes. Model MA024-10

MA230VKIT

Includes surge suppressor, wire jumper, duplex box covers and detailed instructions.

SPRINT OPERATORS ONLY

SPRINT GATE ARM

8' Padded Safety Arm, yellow. Model SP8

REPLACEMENT ARM TUBE

Model SP8 TUBE

REPLACEMENT PAD

Yellow. Model SP8 PAD

WARRANTY POLICY

(YOU MUST READ, UNDERSTAND AND AGREE WITH ALL ITEMS IN THE LIMITED WARRANTY)

LiftMaster warrants the **MEGA ARM-UL** to be free of defects in workmanship and materials for a period of 2 years for electronics and mechanical components and includes a 10 year corrosion perforation warranty on the cover and chassis. Warranty will begin from the date of purchase.

LiftMaster reserves the right of final determination as to the existence and causes of any defect or failure. Any part or parts found to be defective and are returned to LiftMaster within the warranty period, shall at our option be repaired or replaced free of charge F.O.B. the factory. Freight is not included at any time on gate arms and chassis. ONLY UPS ground freight is included during the first year of warranty.

The warranty will not apply the following circumstances which are considered beyond our control.

Mis-use, vandalism, accident, neglect, unauthorized repairs or modifications, acts of God (lightning, floods, insect damage, etc.), power surges, units subjected to corrosive environments, incorrect installation or application, the batteries or incorrect battery installation, operation without or failure to use correct battery type, damage to arm bracket and/or gear reducer due to use of incorrect arm.

The warranty set forth above is entirely exclusive and no other warranty whether written or oral, is expressed or implied. LiftMaster specifically disclaims any and all implied warranties, merchantability or fitness for a particular purpose. It is the purchasers sole and exclusive responsibility to determine whether or not the equipment will be suitable for a particular purpose. In no event shall LiftMaster, inc. be held liable for direct, indirect, incidental, special, consequential damages or loss of profits whether based on contract, tort, or any other legal theory during the course of the warranty or at any time there after. The installer and/or end user agree to assume all responsibility for all liability in use of this product, releasing LiftMaster of all liability.

WARNING! MEGA ARM NOT FOR USE WITH BICYCLES OR PEDESTRIANS. YOU MUST PROVIDE APPROPRIATE SIGNAGE BEFORE ACTIVATING THE UNIT. NEVER ALLOW CHILDREN TO PLAY NEAR OR OPERATE AUTOMATIC GATES.

IN ORDER TO INSTALL AND USE THE MEGA ARM, YOU MUST UNDERSTAND AND BE IN FULL UNCONDITIONAL AGREEMENT WITH ALL STIPULATIONS OUTLINED ABOVE. IF YOU ARE NOT IN FULL AGREEMENT, DO NOT PUT UNIT INTO OPERATION. IF OPERATOR IS PUT INTO OPERATION THIS WILL BE CONFIRMATION THAT YOU ARE IN FULL UNCONDITIONAL AGREEMENT WITH ALL OF THE ABOVE STIPULATIONS.

Materials, components, features and specifications are subject to change without notice.

HOW TO ORDER REPAIR PARTS

OUR LARGE SERVICE ORGANIZATION
SPANS AMERICA

INSTALLATION AND SERVICE INFORMATION
SIMPLY DIAL OUR TOLL FREE NUMBER:

1-800-528-2806

www.liftmaster.com

WHEN ORDERING REPAIR PARTS, ALWAYS GIVE THE FOLLOWING INFORMATION:

- PART NUMBER
- PART NAME
- MODEL NUMBER

ADDRESS ORDERS TO:

THE CHAMBERLAIN GROUP, INC.
Technical Support Group
6050 S. Country Club Road
Tucson, Arizona 85706



4700 Rochester Road,
Troy, MI 48085
248-250-6787(o)
248-462-3907(c)
248-250-6797(f)
mnamin1@msn.com

City of Troy
Planning Department
c/o Brent Savident

**RE: TRAFFIC IMPACT STUDY OF 3635 ROCHESTER RD, JAMIAH MASJID
OF TROY**

Dear Mr. Brent Savident:

In accordance with your request, we have analyzed the proposed new additional exit from 3635 Rochester Road, 1st Jamiah Masjid of Troy, MI 48083 for traffic impact on the surrounding Rochester Road and Colebrook Road. This *letter*, and the conclusions herein, are based on the on-site inspection of the property, exiting and future traffic.

The existing parking lot consists of 145 parking spaces with an additional 6 spaces designated for handicap. The focus of this report essentially has three parts: (1) to define and delineate the exits in its influential area from the current parking lot (2) to identify and analyze traffic flow with regard to the proposed new exit and (3) identify and analyze the impacts of the new flow of traffic from the exit on Colebrook Road with regard to the two exits (one existing and the proposed additional Colebrook Road exit).

The following report presents a narrative review of the assessment and our analysis of facts along with other pertinent materials on which this report is predicated. It contains data and exhibits gathered in our investigations and will include a description of the analytical process and our conclusions.

Traffic studies are a tool that the City of Troy may require to assess how the added traffic from an existing parking lot will affect vehicles, pedestrians, as well as current and future residents. These studies help reviewers make decisions regarding land use zoning, special land uses, subdivisions, and that may negatively impact traffic flow. Usually, a community will set within their ordinance a threshold of new trips generated by the proposed development to determine if a study is required or not.

The size and scope of each traffic study will differ; however, each will generally contain a summary of the proposed development if any, a review of the existing roadway conditions and background traffic operations, discussion of the proposed site and its expected generated trips based on the Institute of Transportation Engineer's (ITE) Trip Generation Manual, an evaluation of the proposed traffic with the existing traffic, and suggested mitigation measures and their effectiveness. It is our interest to communicate the potential impacts of the proposed additional new exit on the traffic, people, and community.

Please refer exhibit F2. Of the Project Location on Rochester Road. Please refer exhibit F6. Of the current traffic volume.

Website link: <https://maps.semcog.org/trafficvolume/>

The traffic on the Rochester Rd near our masjid; daily VMT for segments in map view:42,257 (south bound AADT = 22,700; north bound AADT = 19,300)

EXISTING SITUATION AND ANALYSES

Friday prayer is a mandatory prayer that Muslims hold every Friday (generally occurring for a short period of time between noon from 1:30 PM to 4:00 PM depending on the season). In Jamiah Masjid of Troy this prayer takes place at two different times to accommodate the attending people coming from various office facilities and schools at :

The first congregational prayer start 1:45pm ends 2:30pm while the second congregational Prayer begins at 3:00pm and ends 3:45pm.

Each prayer needs approximately 45 minutes from beginning to end. The existing parking lot becomes almost filled up in 1st Congregational prayer. In the second congregational prayer, the attendances are significantly less not more than 50 to 60 people. The people from 1st congregational prayer must exit the parking lot before 2:45 PM so that the second group can enter to perform their obligated prayer. Having an only manner of entrance and only one exit onto Rochester Road has been posing a problem to perform Fridays congregational prayer management and operations. Currently with only one exit onto Rochester Road it has caused traffic congestion within the parking lot of Jamiah Masjid of Troy at the conclusion of the first congregational prayer.

In order to accommodate the needs of the community, Jamiah Masjid of Troy recommends putting a second exit from the current existing parking lot to the Colebrook Road at the South to alleviate this problem of attending prayers in time. The Colebrook Road exit will not be used as an entrance point and will be structured in such a way as to permit traffic to exit only to the left towards Rochester Road and not into the neighborhood.

For you information, all mosques like IAGD – Rochester Hills– on Auburn Road and Unity Center- Bloomfield —Long Lake Road have double exits.

There is only a traffic signal (Exhibit F6) on Rochester Road approximately 400 ft upstream form the current exit does not help to exit the vehicles from parking lot to Rochester Road using one single exit and entry. There are several Michigan U turn downstream of these exits for people going to the North.

As such, we are recommending one additional exit from the parking lot to Colebrook Road. There is no excess traffic or any increase in traffic volume in the parking lot because there were no additional vehicles or empty parking spaces. These attending people used to come for a limited time permitted by their offices to perform this obligatory prayer.

TRAFFIC IMPACT STUDY (TIS) FOR SUITABLE LAND USE AND TRANSPORTATION

There is no potential risk for a traffic incident i.e., congestion related collisions, non-standard sight distance considerations, increase in traffic conflict points, etc. This TIS requires updating when the amount or character of traffic is significantly different from a previous study. This Rochester Road exit and reentry was in existence since 1996 when the existing building and parking lot were upgraded in 1996 (City of Troy Permit # PB1996-0928).

Therefore, keeping this entry and exit as it was will impose no Traffic impacts to Rochester Road Traffic at the defined times mentioned 2:30 PM to 3:00 PM. In addition, the existing exit and reentry conditions - current year traffic volumes and peak hour level of Service (LOS) analysis on this six lanes Rochester Road will not have any impact on non-peak hours. The existing entry and exit in use by the Rochester Road on weekdays except Fridays and week end will have no significant impact on the Rochester Road.

As a mitigation measure the second recommended exit only to Colebrook Road at South end for allowing less than 10% of the traffic from parking lot (10 to 12 cars) is considered in this traffic impact analysis. This determines this new proposed exit will have impacts that can be eliminated or reduced to a level of insignificance.

The new exit to Colebrook will reduce the traffic congestion at Rochester Road Exit and the exit will reduce this traffic impact to an acceptable level of service (LOS) by adding more safety and LS by installing auto regulated exit gate, turning to left only with additional traffic signals as shown in exhibits F1 and F2.

The current plan of an additional exit can be implemented as there are no increase in the capacity of parking lot and no additional development to the property since 2018, by efficient land use and accommodating only 50 to 60 worshippers' entrance through Rochester Rd exit -entry. The additional Colebrook Road exit can be used to help exiting the parking lot for the 1st group of worshippers. Any future increase in people of second group also this new exit will help to increase the safety of the parking lot and attending people to exit in time of necessity. We hope this site specific TIS will help you to make decision by approving to implement executing of second exit.

Best Regards,

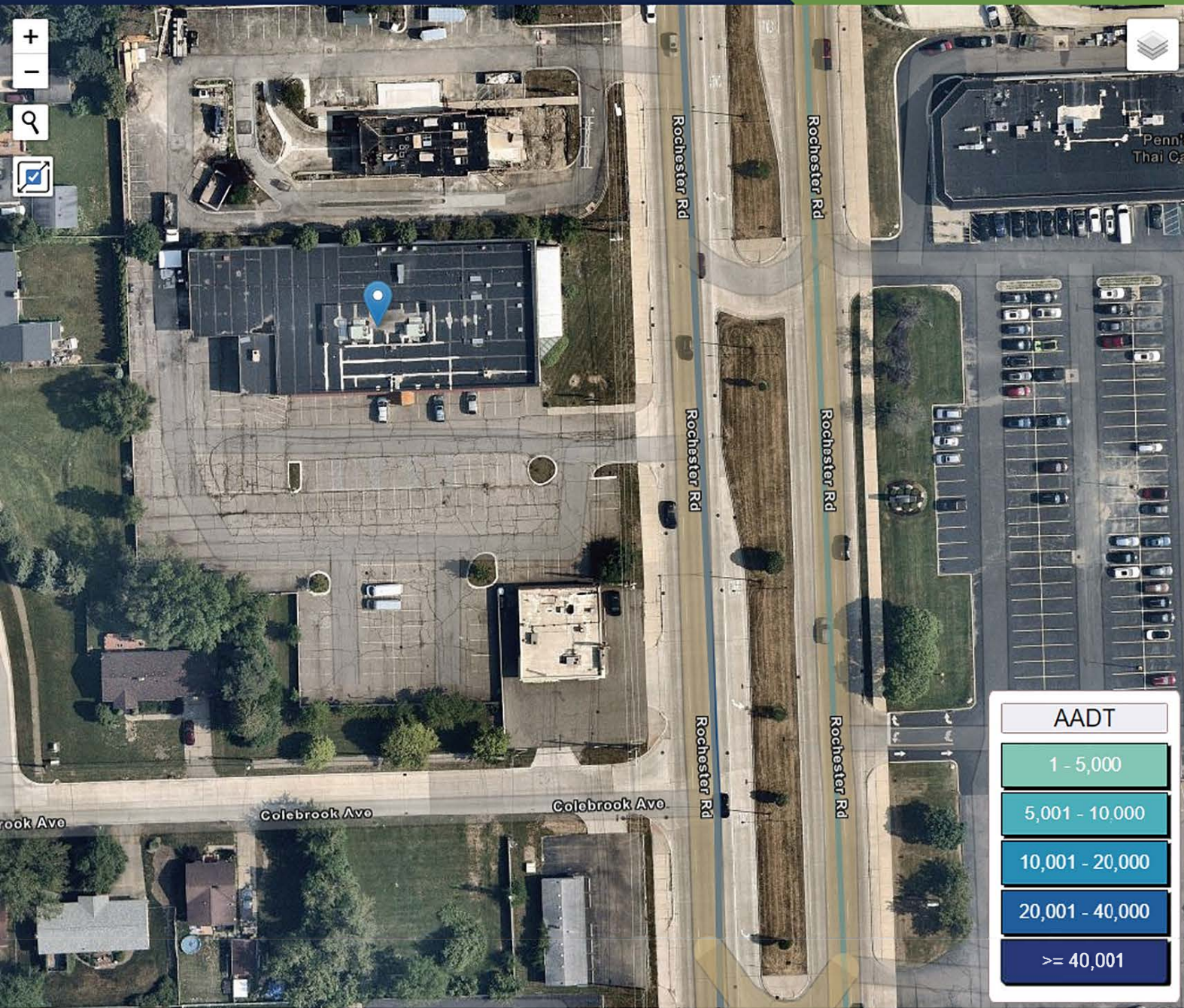


Dr. Md. Nurul Amin PE, Peng.



09/08/2023

NIMA GROUP LLC



Map Video Tutorial ▶

Daily VMT for segments in map view:
42,257

County/Community

7 counties selected

239 communities selected

Latest Count Year

Multiple years selected

Jurisdiction

City, County, State, Uncertified

National Functional Class

6 national functional classes selected

Number of Lanes

6 number of lanes selected

Type

Observed, Interpolated, Default

About

AADT

- 1 - 5,000
- 5,001 - 10,000
- 10,001 - 20,000
- 20,001 - 40,000
- >= 40,001

ITEM #7



Carlisle | Wortman
ASSOCIATES, INC.

117 NORTH FIRST STREET SUITE 70 ANN ARBOR, MI 48104 734.662.2200 734.662.1935 FAX

To: City of Troy Planning Commission
Brent Savident, AICP

From: Ben Carlisle, AICP

Date: November 9, 2023

Re: Master Plan 63-day Public Review Comments

On August 21, 2023, the Troy City Council directed staff to release the Draft City of Troy Master Plan for the 63-day public review period. The 63-day review period is required to allow for public comments. The 63-day period recently ended.

During the 63-day review period, the City received comments from the Oakland County Zoning Coordinating Committee, the Shelby Township Planning Commission, public comments from the Community Center comment station, and emails from six (6) Troy residents. All comments are included in your packets.

Comment Themes:

Resident Comments:

- Lack of affordable housing and housing diversity in Troy
- Overdevelopment, congestion, and traffic
- Bicycle and pedestrian improvements and trail expansion
 - Pedestrian improvements to/from Athens High School
 - Pedestrian improvements at Crooks/Big Beaver and Crooks/Wattles
 - Pedestrian improvements along Big Beaver and Wattles
 - Non-motorized improvements to and around City Civic Campus
 - Bicycle improvements along Beach, Kensington, and Eton Roads
- Add additional “greening” to Troy
 - Encourage native landscape species
 - Remove invasive species from City properties
 - Repurpose vacant and underutilized parking lots
- Repurpose former Kmart Headquarters
 - Central community space with missing middle housing
 - Consider building or donating the property to the arts, such as construction of a Modern Museum of Art with an Art College.

- Repurpose existing sites and buildings to viable uses including residential
 - North Troy
 - Vacant and underutilized office buildings
- Add Farmers Market
- Comments on individual neighborhood nodes

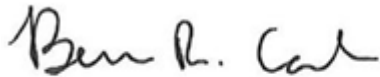
Oakland County Zoning Coordinating Committee:

- The Oakland County Zoning Coordinating Committee found the plan to be Not Inconsistent with surrounding community master plans.
- Recommendations
 - Update Census information
 - Update SMART and Oakland County Transit information in Chapter 5
 - Add Michigan National Features Inventory (MNFI) information into the Master Plan
 - Update name to MDEQ
 - Correct typos in Chapter 9 and 10
 - Incorporate hazard mitigation plan
 - Utilize Oakland County's Neighborhood and Housing Development Divisions varied tools for diversifying a communities housing stock

The Planning Commission is asked to consider these comments and direct staff to make any amendments to the Plan. The City is planning on holding a public hearing on the Master Plan on December 12th.

I look forward to discussing this item in more detail on November 14th

Sincerely,



CARLISLE/WORTMAN ASSOC., INC.
Benjamin R. Carlisle, AICP, LEED AP

October 7, 2022

Commissioner Gwen Markham, Chairperson
Oakland County Coordinating Zoning Committee
1200 North Telegraph Road
Pontiac, MI 48341

SUBJECT: County Code No. MP 23-03, Oakland County Department of Economic Development, Division of Planning & Local Business Development's staff review of the draft Plan Troy 2040 Master Plan Update.

Dear Chairperson Markham and Committee Members:

On September 6, 2023, the Oakland County Department of Economic Development (OCED), Division of Planning & Local Business Development (PLBD) received a mailed letter from the City of Troy (dated August 29, 2023) that initiated the review and comment period for the proposed Draft **Plan Troy 2040 Master Plan Update, (County Code Master Plan No. 23-03)**. Under the Michigan Planning Enabling Act, Oakland County, adjacent municipalities, and other jurisdictional authorities have 63 days to submit comments on Master Plan updates.

This review of the Draft Master Plan will go before the Oakland County Coordinating Zoning Committee (CZC) on October 25, 2023, which falls within the community's specified comment period. It is assumed that adjacent communities were notified about the proposed Draft Master Plan and review period by the City of Troy. The Draft Master Plan can be found at: [CityOfTroy_MasterPlan2040_2023-8-25\(1\).pdf](#)

Staff Recommendation

Based on the review of the surrounding communities' master plans, the Draft *Plan Troy 2040 Master Plan* is **not inconsistent** with the plan of any city, village, or township that received notice of the draft plan. Oakland County has not prepared a countywide development plan, so there is no countywide plan with which to compare the draft amendment. Following is an analysis and summation of the Draft Master Plan update.

Select Summary Analysis of Content

The information included herein represents a summarized analysis of the proposed draft *Plan Troy 2040 Master Plan*. Select sections are highlighted in this review with a focus on changes to borderline conditions and future land uses. County Planning staff last reviewed changes to the community's Master Plan in 2016. The existing Master Plan is referenced as the 2015 plan which was adopted in 2016. Due to Troy being a mostly "built-out" community, much of the proposed document continues to promote relevant goals and objectives from past Master Plans. The City of Troy shares a border with seven (7) adjacent communities which include Bloomfield Township, the City of Birmingham, the City of Clawson, the City of Madison Heights, the City of Rochester Hills, the City of Royal Oak in Oakland County as well as the City of Sterling Heights in Macomb County. Additionally, three (3) communities have oblique (corner) shared borders which include the City of Auburn Hills in Oakland County, Shelby Township, and the City of Warren in Macomb County. The City of Troy is currently the largest populated city in Oakland County and shares borders with some of the largest populated communities in the State of Michigan.

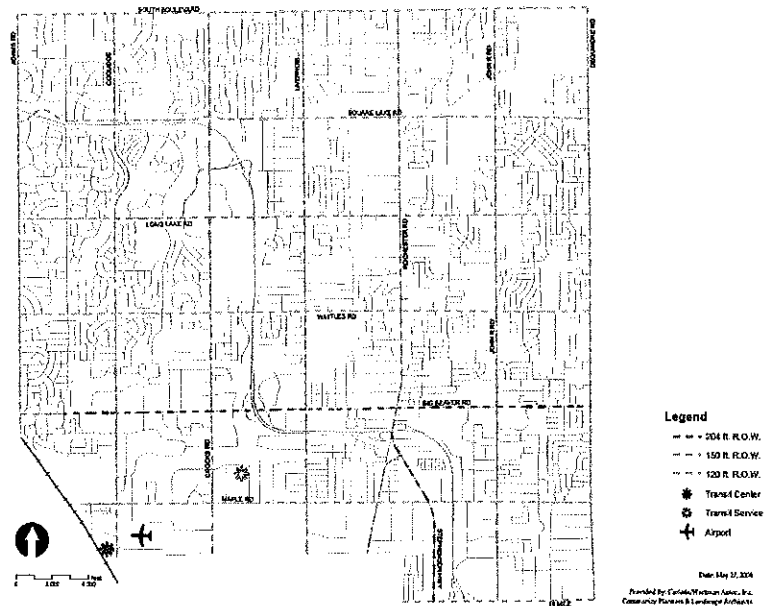
Public Engagement

The city held targeted public input opportunities for the Master Plan update per the information provided in Chapter 1. The plan also continues to utilize and reference relevant data gathered from past public engagement and community participation sessions of 2008 through 2015, including the *Troy Vision 2020* documented information. The public engagement opportunities conducted in 2021 through 2022 were focused on the visions and policies for the "Neighborhood Nodes" that have been present and established in past plans. Per the *Community Participation* portion of Chapter 1 (page 16/ 9 pdf), online and paper surveys were made available to residents and stakeholders. Input was gathered from over 1,650 participants in the spring of 2021. Additionally, the city offered six (6) specific walking tours called *Neighborhood Node Walk & Talks*. The information gathered included aspects of the "Nodes" density, design, zoning, inclusion of open spaces, environment, and landscaping. A Steering Committee with a focus on the "Nodes" of the City was created and consisted of four (4) members of the Planning Commission that met three (3) times throughout the Master Plan Update process to review and report on the outcome and findings of the public engagement process related to the Nodes.

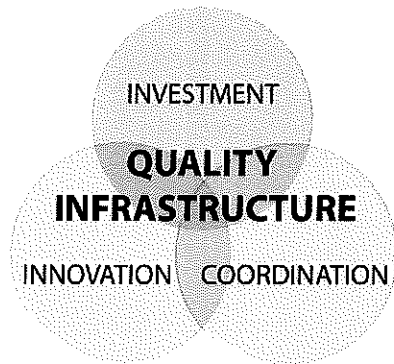
Transportation, Infrastructure & Sustainability

The automobile and the road system promoted the expansive growth of the City of Troy starting in the 1960's. Transportation continues to be an important topic for the city which is prominent throughout the proposed Master Plan and the plans that came before it. The city maintains multiple modes of transportation (automobile, air, rail) and transportation hubs, notably the Troy Transit Center. The Thoroughfare Plan, promoted in Chapter 5, addresses mobility needs and utilizes access management, complete streets, non-motorized systems, and transit-oriented development/hubs as goals in connecting residents and workers to a regional system for unlimited and unrestricted access. The *Big Beaver Corridor Study* continues to support cross-plan discipline within the draft Master Plan as it is referenced many times. The city continues to maintain over 500 miles of sidewalks and 364 miles of road in cooperation with the Road Commission for Oakland County (RCOC). The plan promotes the continuation of the city's ongoing participation in *Faster And Safer Travel Through Routing and Advanced Controls (FAST-TRAC)* through the RCOC and its involvement with the *Suburban Mobility Authority of Regional Transit (SMART)* as the SMART Oakland Terminal is located within the City of Troy.

Thoroughfare Plan



The plan continues to promote and protect infrastructure through investment, coordination, and innovation. Water quality efforts promoted in Chapter 6 of the plan feeds into the environmental sustainability portion of the plan in Chapter 7. The plan has updated the capacity and services utilized through the *Detroit Water and Sewage Department (DWSD)*. Since 2015, the miles of water main in the city has increased from 500 to 550 miles of pipe. Additionally, the number of fire hydrants have increased from 5,300 to 6,100 and the number of water meters have grown from 26,000 to 29,000. The plan promotes coordination with the DWSD, Oakland County Drain Commissioner, and State of Michigan. Water quality remains important to the city in its efforts to provide safe and healthy services.

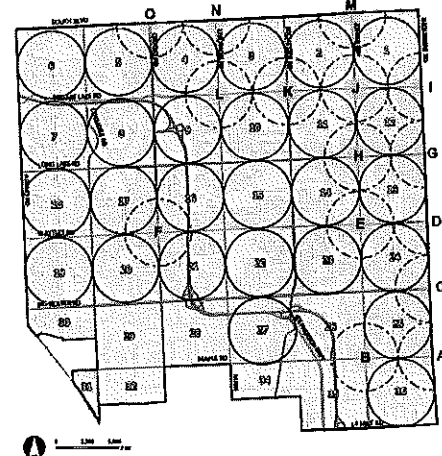
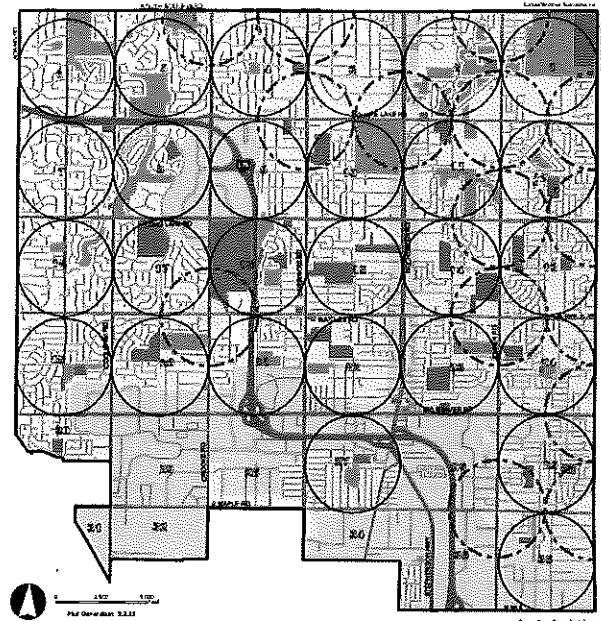
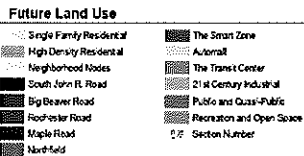
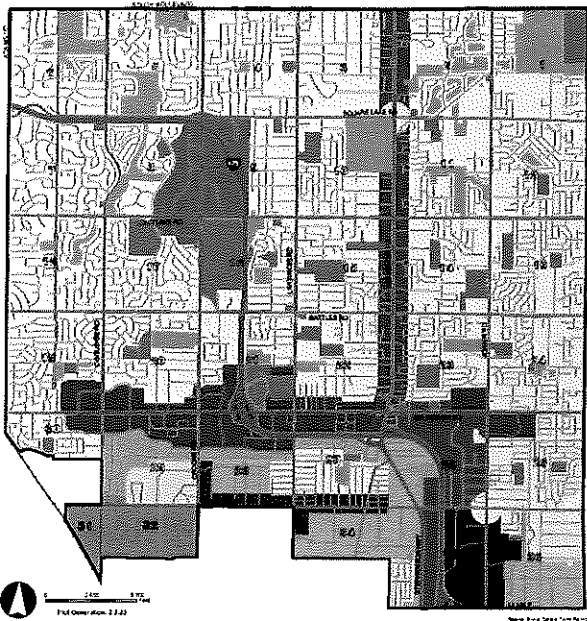


In support of infrastructure protection, quality and maintenance of these systems, the plan promotes sustainable growth through defined "Low Impact Development" (LID) which respects the preservation and enhancement of existing natural features while supporting urban and brownfield redevelopment. Redevelopment initiatives include transportation options (as identified in Chapter 5), green building designs, urban form/neighborhood design, waste water reduction, sustainable design, EV charging

options, and adaptive reuse. An emphasis on *Leadership in Energy and Environmental Design (LEED) Green Building Rating System* continues to be a tool the city uses for new or revitalization projects. Changes in office building use following the Covid-19 Pandemic have created flexibility in adaptive reuse options for the city and property owners. Troy and Southfield have the most office square footage available in Oakland County. Starting in late 2022, Troy is in the process of completing or approving multiple projects that rehabilitate existing, empty office buildings to residential apartments, condos, and other mixed uses along the Big Beaver Corridor. The flexibility in the future land use plan and the opportunity to utilize built structures, which keeps materials out of landfills while helping to provide more missing middle housing (per Chapter 8), promotes sustainability, and will benefit the city of Troy well into the future.

Land Patterns

Chapter 9 *Land Patterns: City Design and Image*, continues to expand the forecast of future land uses to nodes, corridors, and sections, rather than as site specific. However, within each area there remain future development



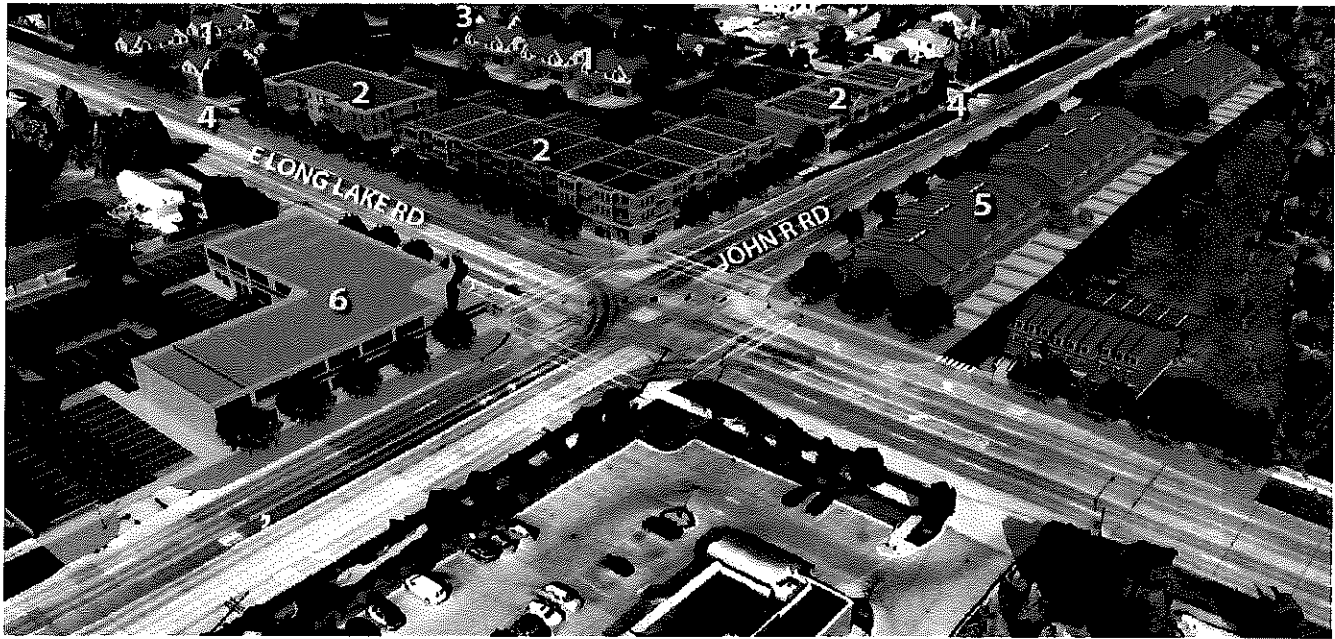
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guides, goals, and objectives as to how the plan intends to shape select areas throughout the city. The proposed Future Land Use (FLU) plan, when compared to the FLU plan of 2015-2016, has few changes. There are no proposed changes to future land uses at any of the city's borders or the description of the land uses that are adjacent to any other community as shown on the provided maps. However, there has been a reduction from 21 to 15 identified "Neighborhood Nodes" at major intersections. The proposed FLU Map and Neighborhood Maps are provided to the right and above (Chapter 9, pg. 86-87/44 pdf and pg. 93/47 pdf).

A majority of the objectives, facts, and design concepts have remained unchanged as compared to the 2015-2016 plan, however, information

has been expanded to be more inclusive of other use types that align with growth patterns and lessons learned in the fallout of the pandemic.

The plan has taken the opportunity to focus on select nodes with modeled concepts that promote sustainable/affordable housing options through density and mixed-use developments while incorporating design attributes and guidelines. Most notably at the Long Lake Rd. / Dequindre Rd. west side of the intersection (Node #G) and Long Lake Rd./John R. Rd. intersection (Node #H) are promoted within the Master Plan.



JOHN R & E LONG LAKE ROADS

The redesigned intersection includes a variety of elements which blend in to the surrounding neighborhood, including:

1. Duplexes
2. Two- to three-story mixed-use commercial and residential

3. A 1.2-acre neighborhood park
4. Curvilinear entry drive connecting Orchard Crest and Wilmet Drives
5. Two- to three-story townhomes along John R with rear facing garages
6. Reconfigured retail including existing pharmacy and new small scale retail tenants

Node-wide design elements for residential:

- Direct pedestrian connections from residential to commercial uses and public sidewalks along the main corridor
- Multiple-row landscape edges provide buffer between land uses

- ▬▬▬▬ FEMA Regulated Floodway (approximate)
- ■ ■ ■ Proposed park boundary

Special Area Plans

Chapter 10, *Special Area Plans* had few changes when compared to the 2015-2016 plan. The "Big Beaver Pedestrian" target area has been eliminated from the Special Area Plan section because many of the 2015 objectives have been implemented while other items have been captured in parts of Chapter 9 under the *Big Beaver Road: A World Class Boulevard* and *The Smart Zone: Big Beaver and Beyond* sections that are located in Chapter 9. Other *Special Area Plans* include a 1-mile section of the Rochester Road corridor, the Maple Road corridor from Coolidge Hwy to John R. rd., and an area identified as "North Troy" consisting of the Long Lake Rd./Crooks Rd. and Interstate 75 area.

These "Special Area Plans" remain unchanged. Priorities surround pedestrian access, traffic safety, and place making. The action plans and implementation strategies remain relevant as some of these specific project areas are currently and remain on a trajectory to follow the plan. Portions of these areas have realized the plan while the other areas within them are still pursuing goals and objectives set forth by this section.

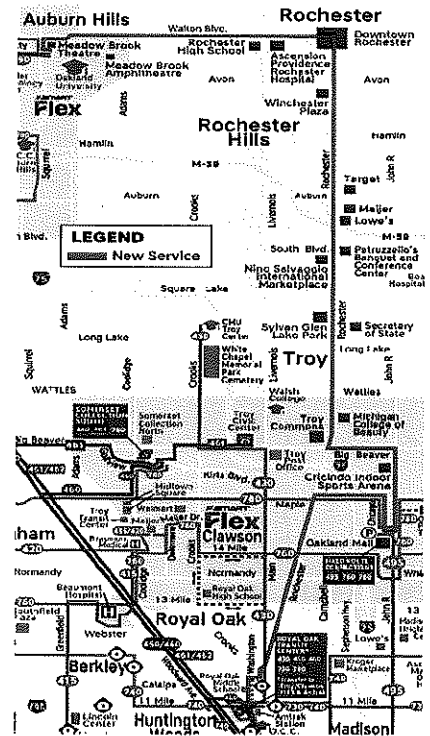
Recommendations

As a function of this review, staff make recommendations in an effort to strengthen the plan so it can be further utilized as a tool for the community. The following recommendations for the Draft Master Plan are listed below:

1. Update Census information and dated measurables. The "City of Troy Facts" have been updated to reflect 2020 Census information but other areas in the plan have not been updated:
 - a. Confirm figure 3.1 has not changed (Chapter 3 pg. 28/15 pdf)
 - b. Consider using recent target/market area statistics and property/industry data in each of the *Special Area Plans* within Chapter 10. Most information is unchanged from the 2015 plan consisting of information from 2013 and 2015. This includes Oakland County Land Use/Property Data which was last updated in 2022. Please include the latest data by visiting: <https://www.oakgov.com/community/community-development/planning-services/current-and-future-land-use-maps-and-statistics> for the most recent information.
 - c. Reference the most current census information available and update the data provided under the "Oakland County" portion of Chapter 4 (pg. 45/23 pdf).

2. Incorporate the following recommendation to enhance Chapter 5 *City in Motion: Local and Regional Mobility*:

- a. Correct conflicting information regarding number of SMART fixed service routes in the city "over 54" (under *Existing Conditions: Transit* pg. 49/25 pdf) as compared to SMART providing "over 41" fixed route services (SMART pg. 51/26 pdf).
- b. Consider adding information pertaining to SMART's recently approved additional route(s) and stops, particularly along the Rochester Road corridor within the City of Troy (see map at right). Such serviced route(s) would start service in January of 2024. Coordinate with SMART staff to incorporate and update information pertaining to SMART services. SMART has additional information relevant to the City of Troy at: <https://www.smartbus.org/Services/Services-By-Community/Troy>
- c. Add Oakland County Transit information pertaining to the approved 2022 Oakland County Public Transit Millage, programs, and resources that could be useful tools for the city's Master Plan. More information can be found at: <https://www.oakgov.com/community/oakland-transit>



3. Add Michigan Natural Features Inventory (MNFI) information in the draft Master Plan. In 2017, MNFI prepared an update to the county-wide data/maps. The City of Troy has designated *Priority II* and *Priority III* areas within the city limits which could help promote the plan's efforts to preserve the City's existing natural features and promote the sustainability initiative as defined in Chapter 3 and supported in Chapter 7, *Green City: Responsibility to Natural & Energy Resources*. Most of the areas in the MNFI data are located along waterways or related to floodplains that are part of the Gibson Drain, Lane Drain, or Ferry Drain systems that are tributaries to the Clinton River. Additionally other sites are located along the River Rouge watershed tributaries and floodplains within the city. Oakland County has online tools where MNFI Maps can be created: <https://oakgov.maps.arcgis.com> or please contact our staff if assistance is required in producing a map for the draft Master Plan.
4. Change the name of the *Michigan Department of Environmental Quality (MDEQ)*, which is referenced in the proposed plan to the *Michigan Department of Environment, Great Lakes and Energy (EGLE)* as the State of Michigan changed that department's name in 2019.
5. Incorporate the following recommendation to enhance Chapter 9 & Chapter 10:
 - a. Future Land Use Categories labeled 75 through 103 do not correspond with the plan's numbering system or page numbers (Chapter 9: pg. 83/42 pdf).
 - b. Update the Neighborhood Nodes maps located throughout the plan (Chapter 9: pg. 89/45 pdf, Chapter 10: pg. 130/66 pdf and maps in Chapter 10 associated with the Maple Road *Special Plan Area*) to accurately account for the number of neighborhood nodes and locations.
 - c. Reorganize, reference and/or change placement of "Neighborhood Node" renderings in the *Neighborhood Nodes: The Economic Neighborhood* and *High Density Residential : Housing*

Choice sections of Chapter 9. Computer generated renderings of select intersections that are titled: *Design Attributes / Guidelines* for Neighborhood Node "G" and "H" and the "City Gateway" renderings do not reference or directly correlate with text/maps before or after the renderings.

- d. Correct numbering issues pertaining to *Gateways, Assets, Opportunities, and Challenges* on maps for the *Rochester Road: Special Area Plan* (Chapter 10: pg. 70/138-139 pdf), *Maple Road: Special Area Plan* (Chapter 10: pg. 79/156-157 pdf), and *North Troy: Special Area Plan* (Chapter 10: pg. 95/188-189 pdf).
6. Incorporate a hazard mitigation plan or elements that could be used in a hazard mitigation plan. Oakland County's PLBD staff is making a deliberate effort to encourage communities to consider the natural and manmade hazards and associated risks to communities and integrate their hazard mitigation priorities and strategies into their Master Plans, when applicable. This is particularly relevant for hazard mitigation related to infrastructure, utilities, natural features, major assets, and historic districts/structures that may be added to the City's Capital Improvement Program if applicable or may represent FEMA (Federal Emergency Management Agency) eligible projects. Through FEMA, communities may be eligible for nationally competitive, annually awarded Building Resilient Infrastructure and Communities (BRIC) and Flood Mitigation Assistance (FMA) grants for planning and project work. Communities responding to and recovering from major disasters or emergencies declared by the President are also eligible for Public Assistance (PA) Funds for emergency work and permanent infrastructure projects. Mitigation opportunities resulting from declared disasters, specifically through the Hazard Mitigation Grant Program (HMGP), are a source of potential funding following major disaster declarations. The 2017 Oakland County Hazard Mitigation Plan is available online through the County's Homeland Security webpage at: <https://www.oakgov.com/community/emergency-management/need-to-know/disaster-planning/hazard-mitigation-plan>.
7. Oakland County's Neighborhood & Housing Development Division has created tools that could strengthen the City of Troy's goals to improve and continue to offer a diversified housing stock while promoting diversity and inclusion in the city's housing efforts. Please go to Oakland County's Neighborhood & Housing Development website: <https://www.oakgov.com/community/neighborhood-housing-development> for more information.

Oakland County Technical Assistance

A summary of programs offered by the Oakland County Economic Development Department that are relevant to the City of Troy has been included on the final page of this review.

Oakland County Technical Resources

Oakland County compiles existing and future land use statistics for the county as a whole and for each community using generalized land use definitions. These documents are included as reference and to provide a snapshot of the City's existing land use and development patterns.

Conclusion Summary

The draft *Plan Troy 2040 Master Plan* remains an all-encompassing and very complete document. The City of Troy remains one of the busiest cities in the county. The population continues to grow, which coincides with the City's ability to attract prominent businesses and keep standards of high education levels state-wide. The Master Plan continues to emphasize the need for pedestrian safety, neighborhood place making with use of nodes, safe and reliable transportation options/availability, and a preservation/protection of the natural features the City has. The plan is well written and has obtainable goals with the incorporation of other Troy Plans listed throughout. Information regarding stakeholders and the support provided through the public input sessions adds value to the plans vision and direction for the City of Troy.

Oakland County does not have a Planning Commission or County Master Plan, so a full comparison and contrast of the information to County-wide Plans is not possible. Our staff review of the proposed Master Plan and a cursory review of adjacent communities' Master Plans have found the City of Troy's Draft *Plan Troy 2040 Master Plan* to be **not inconsistent** with those plans.

Troy has received a copy of this review. Additional copies have been emailed to adjacent municipalities and other reviewing jurisdictions for their review and/or comments. On October 25, 2023, this review will go before the CZC which will consider a motion on the recommendation of the submitted draft Master Plan. If there are any questions or comments about this review and analysis, please do not hesitate to contact me at (248) 858-0389 or email me at krees@oakgov.com.

Respectfully,



Scott E. Kree | Senior Planner
Oakland County Department of Economic Development
Planning and Local Business Development Division

CC: Yolanda Smith Charles, Oakland County Commissioner, District 17 & CZC Vice-Chair
Phil Weipert, Oakland County Commissioner, District 13 & CZC Member
Dave Woodward, Oakland County Commissioner, District 1
Penny Luebs, Oakland County Commissioner, District 2
Gary McGillivray, Oakland County Commissioner, District 3
Brent Savidant, City of Troy Development Director
Benjamin Carlisle, Planning Consultant at Carlisle | Wortman & Associates for the City of Troy
Stephen Cohen, City of Auburn Hills Community Development Director
Nicholas Dupuis, City of Birmingham Planning Director
Patricia Voelker, Bloomfield Township Director of Planning, Building & Ordinances
Nik Stepnitz, City of Clawson Director of Community Development / Assist. City Mngr.
Giles Tucker, Madison Heights Economic & Community Engagement Supervisor
Sara Roediger, Rochester Hills Planning Manager
Joseph Murphy, City of Royal Oak Planning Director
Jake Parcell, City of Sterling Heights City Development Manager / Assistant City Planner
Ronald Wuerth, City of Warren Planning Director
Julijana Misich-Rasaweher, Shelby Township Planning Director
Vicky Rowinski, Macomb County Director of Planning & Economic Development
Brad Knight, RCOC Director of Planning & Environment
Dan Butkus, WRC Engineering Technician, Plan Review & Permitting Unit
Oakland County Airport
Lori Swanson, Oakland TSC-MDOT Manager
Adelaide Pascaris, ITC Area Manager
Jennifer Whitteaker, DTE Regional Manager
Brandon Hofmeister, Consumers Energy Senior VP of Government
Michael Spence, SEMCOG Administrator of Governmental Affairs
CN Rail

Oakland County Planning Resources

The Oakland County Department of Economic Development (OCED), Division of Planning & Local Business Development (PLBD) offers a variety of programs to support Oakland County communities with innovative programming and assistance to create attractive destinations in which to live, work and raise a family. The chart below details those programs offered by the PLBD (a division of the OCED). Current participation in these programs and opportunities for future involvement are noted on the right side of the chart. Additional information on all OCED programs can be found at www.oakgov.com/advantageoakland.

Program	Mission	City of Troy's Opportunities and Current Participation
Environmental Stewardship	Provide information, plans and options to promote conservation of the natural environment while supporting sustainable economic growth, development, and redevelopment.	The City of Troy supports development that is cognizant of natural resource protection and management. County staff members are able to act in a supporting capacity with grant application identification, open space protection, and sustainable development practices as requested.
Historic Preservation Assistance	Support local efforts to maintain and enhance architectural and heritage resources through sustainable practices to enrich the quality of life for all.	County staff is able to assist with potential design concepts for adaptive reuse of any historic structures within the community.
Land Use & Zoning Services	Prepare and provide land use, zoning, and Master Plan reviews for communities to enhance coordination of land use decision-making.	The City of Troy continues to send Master Plan Updates and Amendments to the County for review fulfilling the legislative requirements. Other coordination services are available upon request.
Trail, Water & Land Alliance (TWLA)	Become an informed, coordinated, collaborative body that supports initiatives related to the County's Green Infrastructure Network	The County fully supports the expansion of non-motorized facilities and protection of the natural environment. Oakland County can aid the community in non-motorized planning efforts through education and the identification of potential funding sources.
Brownfield Redevelopment Authority (OCBRA)	Provide assistance in the County's Brownfield initiative to clean-up and redevelop contaminated properties	The OCBRA can assist and coordinate with the State of Michigan Department of Environment, Great Lakes and Energy (EGLE, formally MDEQ) along with the Michigan Economic Development Corporation (MEDC), as needed, in an effort to prepare designated brownfields for redevelopment with the County's BRA.



BOARD OF COMMISSIONERS

1200 N. Telegraph Road, Bldg. 12 East, Pontiac, MI 48341

Coordinating Zoning Committee

Gwen Markham, Chair
October 17, 2023 at 4:00 PM

MINUTES

1. Call Meeting to Order

Chair Gwen Markham called the meeting of the Coordinating Zoning Committee to order at 04:09 PM in Committee Room A 1200 N. Telegraph Road, 12 E, Pontiac, MI 48341.

2. Roll Call

MEMBERS PRESENT: Yolanda Smith Charles, Gwen Markham, Philip Weipert (3)

MEMBERS ABSENT WITH NOTICE: (0)

3. Approval of Minutes

a. Board of Commissioners - dated May 3, 2023

Philip Weipert moved **approval of the Minutes dated May 3, 2023, as presented.** Seconded by Gwen Markham.

Motion Passed.

Vote Summary (2-0-0)

Yes: Philip Weipert, Gwen Markham

No: None

Abstain: None

4. Approval of Agenda

Philip Weipert moved **approval of the agenda, as presented.** Seconded by Gwen Markham.

Motion Passed.

Vote Summary (2-0-0)

Yes: Philip Weipert, Gwen Markham

No: None

Abstain: None

5. Public Comment

None.

6. Communications

- a. **Economic Development - Planning and Local Business Development - Division of Planning & Local Business Development Staff's Review letter to Northville Township for Draft Master Plan Changes**
Philip Weipert moved to **receive and file the attached Communication.**
Seconded by Gwen Markham.

Motion Passed.

Vote Summary (2-0-0)
Yes: Philip Weipert, Gwen Markham
No: None
Abstain: None

7. Regular Agenda

- a. **Economic Development - Planning and Local Business Development - County Code No. MP 23-03, Division of Planning & Local Business Development Staff's Review of the City of Troy Draft Master Plan Update**
Yolanda Smith Charles moved that **based on the review of the City of Troy's Master Plan, "Plan Troy 2040", and the effects it could possibly have on the adjacent Oakland County communities of Auburn Hills, Birmingham, Clawson, Madison Heights, Rochester Hills, Royal Oak, Bloomfield Township and the Macomb County communities of Warren, Sterling Heights and Shelby Township; the proposed Master Plan updates are seen as not inconsistent with those adjacent communities' Master Plans. The proposed changes are acceptable to recommend approval to the City of Troy.** Seconded by Philip Weipert.

Motion Passed.

Vote Summary (3-0-0)
Yes: Philip Weipert, Gwen Markham, Yolanda Smith Charles
No: None
Abstain: None

8. Public Comment

None.

9. Other Business/Adjournment

The meeting adjourned at 04:44 PM.

NOTE: The foregoing minutes are subject to Committee approval.

**Public Comments from Suggestion Box at Draft Master Plan Display in Troy Community Center
(During 63-day public review period)**

- Keep Green Space and parks. Stop building 711 stores and silly things. If anything, affordable housing is needed. Too many strip malls like Sterling Heights. Anonymous
- In regards to the old Kmart headquarters land & property. It would be great if the Forbes / Frankel Families would consider building or donating the property to the arts. A Modern Museum of Art with an Art College. Planning for it done with the DIA Director Salvador Short-pons. Anonymous
- Reinstate Family Night for an \$8 dollar a month resident fee. (No Themes) and hold 2 a month. Also, the \$15 NR fee is a little high just for using the pool during after nights. This will add community participation. Themes can be too much, kids just want to have fun in the pool. Lorraine M.
- Put a changing sign on Livernois describing what's going on in the Troy Community Building. Jean L.
- What a terrible idea to put a carpeting in the lunch room. It will be dirty in a week. Please do not do it. A big light up sign for the Troy Community Center and Senior Center would be a good way to advertise our beautiful faculty. Kathleen B.
- The Troy Community Center is a great place to work and it's easy to stop by the library. Susana W.
- 2295 Rochester Ct. Troy MI 48083 could be converted to a 4-unit condo to improve the image of Troy and provide affordable housing. Ray
- Free Splash Pads like Berkley. Fenced in Plan. Anonymous
- Yes, to improving greenery on Rochester! Central Community space with middle housing at former Kmart HQ.
- Keep Troy Safe for Families. Keep city clean and property values high. Develop responsibly.
- Maintain pickleball 2:00-4:00 novice inter.
- I would love to see 1 or 2 music rooms. I would pay to use a music room for guitar or piano. There was an entire music building at Ferris State U. That was used all the time for music study.



Planning & Zoning

52700 Van Dyke Avenue
Shelby Township, MI 48316-3572
Phone (586) 726-7243
Fax (586) 726-7227
planning@shelbytp.org
www.shelbytp.org

September 28, 2023

City of Troy
Attn: Brent Savidant, AICP
500 West Big Beaver
Troy, MI 48084

RE: CITY OF TROY DRAFT MASTER PLAN

Dear Mr. Savidant,

On Monday, September 25, 2023, the Charter Township of Shelby Planning Commission discussed the proposed City of Troy Draft Master Plan. A copy of the draft minutes is enclosed.

Please feel free to contact me in the event that you have any questions. Thank you for your time and cooperation.

Sincerely,

CHARTER TOWNSHIP OF SHELBY

Julijana Misich
Planning Director

/kp

Encl

RECEIVED
OCT - 2 2023
PLANNING

DRAFT MINUTES OF THE CHARTER TOWNSHIP OF SHELBY PLANNING COMMISSION REGULAR MEETING HELD SEPTEMBER 25, 2023, IN THE SHELBY TOWNSHIP MUNICIPAL BUILDING, BOARD ROOM, 52700 VAN DYKE AVENUE, SHELBY TOWNSHIP, MICHIGAN, 48316.

Planning Commission Chairman Moffitt called the meeting to order at 7:00 p.m.

PC Members Present: Jerome Moffitt, Lisa Casali, John Snider, Phil Turner, Raquel Moore, Lisa Baker, Gene Meredith

Members Absent: Salvatore Lore, Anthony Apone

Also Present: Julijana Misich-Rasaweher, Planning Director
Nick Fiore, Senior Planner
Robert Kirk, Township Attorney
Kristi Pappas, Administrative Secretary
35 people in the audience

APPROVAL OF AGENDA

Amendment made to the agenda to include Macomb Township Meijer Express Gas Station correspondence under New Business.

Motion by Turner, supported by Snider, to approve the amended agenda as submitted. Motion carried.

APPROVAL OF MINUTES

Motion by Snider, supported by Meredith, to approve the minutes of September 11, 2023, as submitted.

Motion carried.

CORRESPONDENCE:

CITY OF TROY DRAFT MASTER PLAN

Misich stated the City of Troy is providing a copy of their proposed draft Master Plan. Shelby Township shares a boundary line at the southwest corner of M-59 and Dequindre Roads. The Troy Draft Master Plan Future Land Use map identifies their quadrant as Recreation and Open Space and Public and Quasi-Public space. The proposed land use designations would not have an adverse effect to Shelby Township and deem the compatibility with planning and zoning policies along our common boundaries are consistent with the Township.

MOTION:

Motion by Snider, supported by Baker, to support the adoption of the City of Troy Master Plan.

ROLL CALL VOTE:

AYES: Snider, Baker, Meredith, Casali, Moore, Turner, Moffitt

NAYES:

Motion carried.

SITE PLANS/PUBLIC HEARINGS:

SITE PLAN #23-32 JOE YOUNGBLOOD (Shelby Township Parks & Recreation); Vacant Parcel No. 23-07-20-300-043, located south of 22 Mile Road, on the east side of Ryan Road; River Bends Park – Kayak Launch

The applicant and/or representative(s) were not present.

Fiore stated the Township is proposing a kayak launch at Riverbends Park for access to the Clinton River. The general parking area is proposed to consist of gravel and handicapped

From: [Planning](#)
To: [Brent Savidant](#)
Subject: FW: City plan
Date: Wednesday, October 11, 2023 8:17:23 AM

Jackie Ferencz

Office Manager |
City of Troy Planning Dept

O: 248.524.3364

-----Original Message-----

From: Donna Santoro <dlsantoro59@gmail.com>
Sent: Tuesday, October 10, 2023 7:24 PM
To: Planning <planning@troymi.gov>
Subject: City plan

CAUTION: This email did not originate from within the City of Troy. Do not click links or open attachments unless you recognize the sender and know the content is safe.

I live in the stone ridge neighborhood in Troy. You have every corner of our square mile built up beyond recognition. I don't want to see these two or three story townhouses developed. You say you want to preserve our green spaces this doesn't achieve that. It's going from a feeling of wide open spaces to congestion. Not to mention the traffic impact which means widening the roads. No Thank You!

So disappointed in this plan. Please scrap much if this. My disappointment will be reflected in my vote.

Donna Santoro

From: [Planning](#)
To: [Brent Savidant](#)
Subject: FW: 2040 Master Plan thoughts/questions
Date: Tuesday, September 12, 2023 9:38:44 AM
Attachments: [image001.png](#)
[image002.png](#)
[image003.png](#)
[image004.png](#)
[image005.png](#)
[image006.png](#)



Jackie Ferencz
Office Manager |
City of Troy Planning Dept
O: 248.524.3364



From: Jasper Gill <drjaspergill@gmail.com>
Sent: Tuesday, September 12, 2023 9:10 AM
To: Planning <planning@troymi.gov>
Subject: 2040 Master Plan thoughts/questions

CAUTION: This email did not originate from within the City of Troy. Do not click links or open attachments unless you recognize the sender and know the content is safe.

1. Wattles/Crooks node: This node has a wonderful restaurant and convenient store now as well as apartments. However getting to this node from the West Troy neighborhoods around Beach road remains difficult since Wattles lacks continuous sidewalks. How can the city make this an actual node for residents so that we could walk or bike there for the adjacent neighborhoods along Adams and Crooks. Wattles desperately needs connecting sidewalks since it is completely residential from Adams to Rochester.
2. More bike lanes would be a wonderful addition to the city. Currently there are a massive amount of bikers using Beach, Kensington, and Eton road. Amongst these safety is most on Eton. Can you imagine the benefit to both kids and adults with a protected bike lane along Wattles that would connect residents to the Rochester road corridor businesses as well.
3. Big Beaver development is fantastic. Vacant office buildings and massive parking lots contribute to crime as unwatched spaces. Developing these into high density residential and businesses supports our economy, tax base (that we will have to make up once companies don't resign their commercial leases), and provides workers for jobs. From the entire stretch of the DDA, will the sidewalks be widened to accommodate more pedestrians? This really is an opportunity to have a special boulevard like the Detroit Riverwalk if done properly. Also a huge economic development.

4. Troy trails are nice but without them connecting they remain paths to nowhere. Citywide connection east to west would be beneficial. Could parking lots be repurposed in being partially used as connectors for the trails?

5. Focusing on green space should also focus on reuse of the massive sea of parking lots our city has accumulated. A greener focus here would help with our flooding issues in the city.

6. The national demand for single family homes is very apparent with home prices. Office buildings are not coming back. Investing in our internet fibers would make Troy an ideal residential community when so many jobs will be remote once office leases expire and companies decide to be lean and save billions on real estate. This should be a priority.

7. Reusing these sites along Long Lake and Crooks (and all North Troy office parks) should also focus on single family residential. Residents are okay with Big Beaver being our downtown with apartment and condominium buildings and major entertainment businesses. But this north corridor should shift to single family homes as reuse of these massive parking lots and vacant apartment buildings.

8. The Big Beaver and Crooks intersection is becoming a wonderful "downtown Troy" Street front businesses on both sides of Big Beaver are developing with great restaurants and a great look. It's positioned perfectly between Downtown Clawson and Downtown Birmingham as well. But how are we connecting both sides of the street? There should be some street planning here so that you can cross Big Beaver to access both sides of the businesses and a safe car flow through this area as well.

9. The Troy Civic Center area needs to be accessible to residents both east and west by non-motorized means. Residents living on Coolidge that want to go to the library or City Hall should be able to safely get there without having to drive. Could there be shuttle service or bike lanes along Wattles for residents to access here?

10. Farmer's Market. Why not have a beautiful large shed like Royal Oak or even Eastern Market? We are situated in the heart of the Metro and have ample space now in the Civic Center. We should have a large convenient year round market for Farmers to deliver their goods for residents who want quality and freshness.

Thank you for your time,

Jasper Gill
3120 Newport CT, Troy MI 48084

From: [Planning](#)
To: [Brent Savidant](#)
Subject: FW: City of Troy Master Plan Feedback
Date: Wednesday, October 25, 2023 1:00:07 PM
Attachments: [image001.png](#)
[image002.png](#)
[image003.png](#)
[image004.png](#)
[image005.png](#)
[image006.png](#)

Not sure if this was forwarded to you when I was out. It came to the Planning inbox.



Jackie Ferencz
Office Manager |
City of Troy Planning Dept
O: 248.524.3364



From: Russell R <russell.reynolds1@gmail.com>
Sent: Tuesday, October 17, 2023 2:01 PM
To: Planning <planning@troymi.gov>
Subject: City of Troy Master Plan Feedback

CAUTION: This email did not originate from within the City of Troy. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Hello,

I had a few points on the proposed Master Plan.

1. The proposed Master Plan mentions native plants a few times, but should go further in specifying that they are essential for wildlife, including birds, mammals, bees, and butterflies including monarchs. Native plants will have a longer lifespan, be more drought tolerant (thus reducing water consumption), help with water retention which reduces flooding, and is more beneficial ecologically, so they should have a more prominent position in this Master Plan. Requiring all new plantings (especially trees) should be considered to help make Troy more vibrant and a greener city.
2. The proposal doesn't at all deal with the issue of invasive species that are taking over many of the city parks and city owned properties, along with many other areas of the city. The area behind the aquatic center is approximately 85% invasive species (mostly Teasel and buckthorn). I recently contacted the city about this and they said they didn't have the budget to address these issues. This must be addressed in the master plan for this reason.

Thank You,
Russell Reynolds

From: [Planning](#)
To: [Brent Savidant](#)
Subject: FW: Comments on Troy master plan
Date: Wednesday, October 11, 2023 9:21:02 AM

Jackie Ferencz

Office Manager |
City of Troy Planning Dept

O: 248.524.3364

-----Original Message-----

From: Jessica Wernette <j.l.wernette@gmail.com>
Sent: Wednesday, October 11, 2023 8:21 AM
To: Planning <planning@troymi.gov>
Subject: Comments on Troy master plan

CAUTION: This email did not originate from within the City of Troy. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Hello,

As a Troy resident of almost 10 years with my husband and two young kids, who decided to move to Troy for its school system (which I'm currently not too happy with) parks, amenities and affordable living, I wanted to provide some comments on your master plan.

1) Affordable housing - Not only is the current state of our economy and sky rocketing interest rates suppressing new home buying power, but the ability to secure or upgrade into affordable, new, family homes in Troy is completely unattainable except for the select few. You have allowed Mondrain to dominate the new housing market with their \$740k starting point homes and blocked almost all other more affordable and better quality builders from entering Troy. With over 90% of Troy developed, I'm afraid there is likely no chance that my family can stay in Troy and upgrade into a new home for our growing family. We will be forced to leave. Take a page from Plymouth, Washington Township, Utica, Sterling Heights, or Macomb. They have been able to achieve allowing builders like Lombardo and Robertson come in and secure appropriately sized lots (not these 6 foot x 10 foot backyards that Mondrian builds for), with a growing family sized house footprint (2000-3300 sq ft) with an incredibly desirable starting price point that is more achievable to your residents. It is clear Mondrian is not diverse in their offerings or target audience. By allowing them to continue dominating the new home marketing in Troy, you are pushing our a whole demographic of other young families who wished to stay in Troy and upgrade but now can't. Think what mid 30s aged couple can afford a new home with 4 bedrooms and a big enough floor plan for a growing family that averages \$820k...not many. Not many at all. You are pushing us all out! Their homes are also incredibly low quality and their building practices are sub par at best. Just look at all the complaints us existing residents who lived behind their 2020-2022 development on Long Lake and John R had to say about the disruption and damage to our properties their new builds caused. I encourage you to think about how to offer affordable new single family, two story homes to young families here in Troy, of all backgrounds, without driving us out with the current limited and

diversely lacking options.

2) Your proposed plan for John R and Long Lake will not be met with welcomed, opened arms. We have enough congestion and traffic being that we are a direct route to the hospital. This is the one small area of Troy that has avoided mass commercialization and I'm not sure what executing your proposed plan of mass commercializing this area solves...except creating more traffic and more congestion for those of us who live here. Creating this high density housing is not addressing the need for affordable single family homes. I assure you those of us who live over here will be fighting back and fighting back hard, or just straight up leaving Troy. The constant need to over-congest Troy is getting out of hand and I think you really need to hold a forum with your long time, or growing young family residents to understand what we want. This is not it. We want space for our children...not a shopping mall on every corner. The great thing about Troy is it already has everything we need working a close distance...we don't need more. We need and want more open space.

There are several things in this plan I like including; ensuring nature preservation (however I'm not sure how you can do that while building over open land...?) and I like the development of community outdoor spaces and activation areas. I love going to Royal Oak and Birmingham because they have walkable downtown areas mixed with retain, entertainment and parks/outdoor space.

An additional request I would ask is considerations for how this plan keeps city and property taxes manageable without drastic YOY increases. Lower taxes is another reason people are so drawn to Troy as compared to places like Rochester, Birmingham and Bloomfield Hills.

And one final lighthearted request...can we please get a Trader Joe's in Troy. Please!

Happy to discuss my comments further.

Thank you

Jessica Brideau (Wernette)

248.880.1200

From: [Planning](#)
To: [Brent Savidant](#)
Subject: FW: Safe Pedestrian access to Athens High School- Master Plan Input
Date: Friday, October 27, 2023 1:49:56 PM
Attachments: [image001.png](#)
[image002.png](#)
[image003.png](#)
[image004.png](#)
[image005.png](#)
[image006.png](#)



Jackie Ferencz
Office Manager |
City of Troy Planning Dept
O: 248.524.3364



From: Steven Roadknight <steven.roadknight@gmail.com>
Sent: Friday, October 27, 2023 12:23 PM
To: Planning <planning@troymi.gov>
Subject: Safe Pedestrian access to Athens High School

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The masterplan needs to include improving pedestrian access to Athens from the south side of Wattles road.

The side walk on the southside of Wattles has areas that are not paved at all. Coming from Dequindre approaching John R there is a section between Anvil and Wayfarer that you have to basically walk on the road.

High school students who live south of Wattles between Dequindre and John R have no safe way to walk to school.

I see 2 options to improve student safety.

1. Complete the side walk to extend all the way to John R
2. Install a crosswalk with traffic lights around north end of Forge or Gate drive to enable students to safely cross to the North side of Wattles where there is completed side walk.

Thank you for attending to this safety concern.

From: [Planning](#)
To: [Brent Savidant](#)
Subject: FW: Troy Master Plan Feedback
Date: Thursday, November 9, 2023 12:32:53 PM

Would this kind of email go to you sir?

Dax Clarke

Housing & Zoning Inspector
City of Troy

O: 248.524.3365

C: 248.561.6418

-----Original Message-----

From: Mark D Barbieri <mark.d.barbieri@gmail.com>
Sent: Wednesday, November 8, 2023 11:16 PM
To: Planning <planning@troymi.gov>
Subject: Troy Master Plan Feedback

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I am a long time resident of Troy having lived here from birth until 2001 and now again since 2013. Over that span, I have lived in 5 different homes and neighborhoods and worked in 3 different office locations at 16/Livernois, 18/Crooks and 19/Crooks.

After reviewing the proposed master plan, some areas are fine others are out of touch and disappointing. In general, we are overdeveloping Troy and cramming buildings anywhere we can. Is especially noticeable when tiny "streets" with a dozen homes at most pop up in small spaces, or when "street walls" are used. Maintain as much undeveloped space as possible throughout the city. That is a major characteristic of Troy and a big draw for residents. The worst proposals are for corners G) 18/Dequindre, H) 18/ John R, M) South/John R.

Also of note the road layouts that were done near I 75 on 16 mile and 14 mile are the dumbest thing I have ever seen. Whoever proposed, designed or approved the traffic flow to cross back and forth with oncoming traffic should have been immediately fired. It was a huge waste of tax dollars and should be demolished and put back. Every time I drive those areas I think of the academic contest scene in Billy Madison with the quote "What you've just said is one of the most insanely idiotic things I have ever heard. At no point in your rambling, incoherent response were you even close to anything that could be considered a rational thought. Everyone in this room is now dumber for having listened to it."

Focusing on the Land Patterns section

B) 15 / John R. Improved commercial would be good here. Corner has languished for years, business have been difficult to sustain, maybe because it is sandwiched between high traffic 14 mile and 16 mile.

C) 16/Dequindre. continued investment in the commercial northwest would be good, but there is almost too much customer volume for the available parking. Improved accessibility is needed.

D) 17/Dequindre. Residential on northwest corner could be ok only if done well. Frankly though it would be best to keep much of the area green.

E) 17/John R. The mix of uses sounds right, but any redevelopment should fit within the current developed footprint. Don't expand out from the corner.

F) 17/Crooks. One of the few corners largely undeveloped. Some development would be good, but would like to see half of it stay green.

G) 18/Dequindre. I agree with the northwest corner use, except for the "street wall" concept. This concept makes the city feel more urbanized and crowded while less suburban, open and green. Keep parking along the road instead of 3 story buildings crowding the sidewalk. The southwest corner though doesn't need a tiny street of condominium. These tiny developments are so ad hoc and misplaced. They truly are just shoehorned into small pockets of land that is undeveloped. Just leave it undeveloped. The multi story mixed retail is fine, though we currently have too much of this space that sits idle at that corner. If needed in the future, just don't use it to make a street wall.

H) 18/John R. The proposed intersection should be entirely scrapped for both the northeast and northwest corners. Larger multi-family units, connecting Orchard Crest and Wilmet should not happen. The corner is currently one of the best residential corners in Troy. Green and open should be maintained in the northwest. Any redevelopment in the northeast should remain small, not much bigger than it is today. Frankly the housing development currently underway is also a mistake for the area.

J) 19/John R. Kensington did a nice job with blending construction with nature and openness. However the invasive phragmites are a problem along John R.

K) 19/ Rochester. Three existing commercial corners should be maintained and improved. The southeast is lacking and has good potential.

L) 19/Livernois. The idea of the townhouse was good, but poorly executed. They are too close to the road, too close to Peppys and aesthetically dissonant. Would like to see moderate expansion /redevelopment of the northeast corner and maintaining footprint of the northwest and southeast.

M) south/John r. The idea here is completely off base. Cramming in multi family unit housing in the southwest corner is wrong. Leave the homes as they are. The massive road/ trail expansion was a waste and detrimental to the existing homes. There is no need for a "gateway" when John R dead ends into Bloomer park in just a couple miles. The southeast corner however needs help and could use something to complement the office spaces along south Blvd.

Mark Barbieri
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